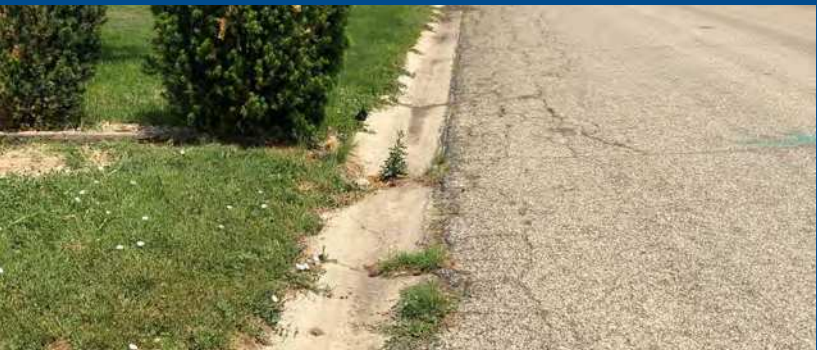


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# MERIDIAN CDBG PROGRAM

## *LMA Walkability Study*

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*Completed July 2023*

<https://meridiancity.org/cdbg>

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# Analysis~Identification of Future Sidewalk & Streetlight Projects (CDBG) Analysis and Recommendations

**Date:** June 30, 2023

**Project:** Analysis~Identification of Future Sidewalk & Streetlight Projects (CDBG)

**To:** Crystal Campbell  
City of Meridian, Community Development Program Coordinator

**From:** Hannah Long, PE  
Conсор, Project Engineer

**Reviewed By:** Jonathan Gellings, PE  
Conсор, Principal Engineer

**Re:** Analysis and Recommendations

---

## Introduction

The City of Meridian (City) receives community development block grant (CDBG) funding from the United States Department of Housing and Urban Development each year, which is allotted to various public facilities and infrastructure improvements, housing, and public services for low to moderate income (LMI) City residents. The goal of this study is to identify public infrastructure projects within LMI neighborhoods that are eligible for future CDBG funding and would provide improved accessibility and safety to the community. Potential improvements include sidewalk infill, ADA ramp installation, and street lighting infill. The purpose of this memorandum is to present the results of the data collection that has been performed for potential project areas and recommend projects for future City CDBG funding.

## Evaluation of Potential Projects

### Criteria

The project selection criteria are detailed in the “Project Selection Criteria” memorandum (**Appendix A**) previously submitted to the City. In summary, the project selection criteria are divided into three categories: pedestrian demand, pedestrian infrastructure, and safety. The criteria in each category are listed below with their method of evaluation, either in-office, field visit, or both:

- Pedestrian demand:
  - Walkability index (in-office)
  - Proximity to school (in-office)
  - Proximity to park (in-office)
  - Proximity to a transit stop (in-office)
- Pedestrian infrastructure:

- Sidewalk continuity (both)
- ADA compliance (both)
- Safety
  - Level of crime (in-office)
  - Crash history (in-office)
  - Street lighting (both)

Detailed metrics and scoring for each criterion can be found on the rubric in **Appendix B**.

## Method

To develop an initial list of potential projects, Consor primarily analyzed sidewalk continuity, as this metric is highly important for improving accessibility and safety for pedestrians. Areas where there is no continuous sidewalk on either side of the road were considered high priority, followed by areas with continuous sidewalk on only one side of the road. Consor then looked at patterns in the other metrics to narrow down the sidewalk infill locations. Locations with higher historical reported crimes, closer to schools, parks, and transit stops, and with improvement needed to the street lighting system were selected as potential projects. Before finalizing the list of potential projects, Consor used the Ada County Highway District (ACHD) Integrated Five Year Work Plan map ([ACHD 2022-2026 Integrated Five Year Work Plan \(IFYWP\) \(arcgis.com\)](#)) to identify ACHD projects with associated pedestrian improvements and eliminate overlap with potential CDBG projects.

These initial focus areas were not limited to what could be constructed in a single CDBG funding year; many segments are larger or smaller than the anticipated size of a CDBG-funded project. The potential project areas identified for analysis are shown in Table 1 below. Additionally, the map of these locations can be found in **Appendix C**.

*Table 1: Potential Project Areas*

Project Area Identifier	Potential Project Area Description
A	NW 7 <sup>th</sup> St from W Carlton Ave to W Cherry Ln
B	W Carlton Ave from approximately 130 ft west of NW 7 <sup>th</sup> St to NW 5 <sup>th</sup> St
C	W Carlton Ave from NW 5 <sup>th</sup> St to NW 4 <sup>th</sup> St
D	W Maple Ave from NW 4 <sup>th</sup> St to NW 2 <sup>nd</sup> St
E	W Carlton Ave from NW 1 <sup>st</sup> St to N Meridian Rd and NW 1 <sup>st</sup> St from W Carlton Ave to 100 ft south of W Washington St
F	NW 3 <sup>rd</sup> St from south end to W Pine Ave and W Broadway Ave from NW 4 <sup>th</sup> St to NW 2 <sup>nd</sup> St
G	NW 2 <sup>nd</sup> St from W Railroad St to Meridian Elementary School
H	NW 1 <sup>st</sup> St from W Railroad St to W Broadway Ave
I	E Carlton Ave from N Meridian Rd to 120 ft west of N Main St
J	E Badley Ave from NE 2 ½ St to Creekside Arbour Apartments and E 3 <sup>rd</sup> St from E Badley Ave to south end
K	E Washington Ave from NE 2 ½ St to NE 4 <sup>th</sup> St
L	E Carlton Ave from NE 2 ½ St to east end
M	E State Ave from NE 2 <sup>nd</sup> St to east end (N Cathy Ln)
N	E 4 <sup>th</sup> St from E Pine Ave to E Carlton Ave
O	E 5 <sup>th</sup> St from E Pine Ave to E Washington Ave

P	Jackson Drain crossing between E Barbara Dr and multi-use path parallel to N Cougar Wy/E Willowbrook Dr
Q	W Pennwood St from Pennwood Apartments to S Meridian Rd

### Analysis

Conсор analyzed the potential project areas A through Q using the project selection criteria rubric and the results of this analysis are displayed in Tables 2 and 3 below. Table 2 shows the unweighted score and Table 3 shows the weighted score and total score for each potential project area. As documented in the Project Selection Criteria memorandum, certain criteria are weighted more due to higher City priority and relevance to the types of projects eligible for CDBG funding. **Appendices D through F** contain exhibits and supporting data for the pedestrian demand, pedestrian infrastructure, and safety categories, respectively.

*Table 2: Potential Project Scores, Unweighted*

Project Area	Walkability Index	Proximity to School	Proximity to Park	Proximity to Transit Stop	Sidewalk Continuity	ADA Compliance	Level of Crime	Crash History	Street Lighting
A	2	4	0	3	4	4	2	1	4
B	2	3	0	3.5	2	0	2	0	4
C	2	3.5	0	3	2	0	2	0	2
D	2	3	1	3	4	4	3	0	2
E	2	4	0.5	4	2	2	4	0	2
F	2	3.5	0	4	4	4	3	0	2
G	2	3.5	0	4	4	4	4	0	2
H	2	3	0	4	4	4	4	0	2
I	3	1	1	4	2	0	4	0	3
J	3	0.5	2	3	4	4	2	1	2
K	3	0.5	2	3.5	4	4	2	0	2
L	3	0.5	1.5	4	3	4	3	0	2
M	3	0.5	1	4	1	0	3	0	3
N	3	0	1	4	4	4	3	0	2
O	3	1	1.5	4	2	0	3	0	2
P	3	3	4	2.5	4	4	2	0	4
Q	2	1	0	1	2	3	3.5	2	3

Table 3: Potential Project Scores, Weighted

Project Area	Walkability Index	Proximity to School	Proximity to Park	Proximity to Transit Stop	Sidewalk Continuity	ADA Compliance	Level of Crime	Crash History	Street Lighting	Total Score	Rank
	x 1.0	X 1.0	X 0.7	X 1.0	X 1.5	X 1.5	X 1.0	X 1.0	X 1.0		
A	2	4	0	3	6	6	2	1	4	28.0	2
B	2	3	0	3.5	3	0	2	0	4	17.5	14
C	2	3.5	0	3	3	0	2	0	2	15.5	17
D	2	3	0.7	3	6	6	3	0	2	25.7	6
E	2	4	0.35	4	3	3	4	0	2	22.4	11
F	2	3.5	0	4	6	6	3	0	2	26.5	5
G	2	3.5	0	4	6	6	4	0	2	27.5	3
H	2	3	0	4	6	6	4	0	2	27.0	4
I	3	1	0.7	4	3	0	4	0	3	18.7	13
J	3	0.5	1.4	3	6	6	2	1	2	24.9	7
K	3	0.5	1.4	3.5	6	6	2	0	2	24.4	9
L	3	0.5	1.05	4	4.5	6	3	0	2	24.1	10
M	3	0.5	0.7	4	1.5	0	3	0	3	15.7	16
N	3	0	0.7	4	6	6	3	0	2	24.7	8
O	3	1	1.05	4	3	0	3	0	2	17.1	15
P	3	3	2.8	2.5	6	6	2	0	4	29.3	1
Q	2	1	0	1	3	4.5	3.5	2	3	20.0	12

## Future Project Scoping

The potential project area evaluation in the previous section informs which areas should be prioritized for future CDBG funding. However, most segments on the list contain more improvement work than can be completed with the average amount of CDBG funding per year. Therefore, the potential project areas must be condensed into right-sized projects that will provide a community benefit in line with the CDBG program goals and will be achievable within the limits of the available CDBG budget.

### CDBG Eligibility

Each project recommended will align with the CDBG program goals for the public facilities and infrastructure category. These goals include adding community value to a designated LMI neighborhood, specifically in locations that are primarily residential. Each project will fall within the scope of a 03K (Street Improvements) or 03L (Sidewalks) project. Additionally, each project will be comprised primarily of new sidewalk, ADA ramp, and/or street lighting construction, with rehabilitation of existing infrastructure only as necessary where attaching to existing. This will place the selected projects beyond and outside the scope of routine City maintenance responsibilities which may include repairing existing sidewalks and replacing expended street light bulbs.

## Project Cost Estimating

Conсор put together unit cost information based on recent projects and publicly available bid item prices to create planning-level costs for typical project elements. The city provided an assumed annual CDBG construction budget of \$200,000. The planning-level project costs are listed below and include a significant contingency budget to account for unanticipated costs such as drainage, utility adjustments, and landscaping.

- Design: \$40,000
- Construction: \$160,000
  - Mobilization (10%): \$16,000
  - Traffic Control (8%): \$12,800
  - Erosion Control (2%): \$3,200
  - Survey (2%): \$3,200
  - Removals (5%): \$8,000
  - Contingency (20%): \$32,000
  - Sidewalks: \$85 per linear foot assuming 5' wide
  - Curb Ramp: \$2,000 each
  - Curb and Gutter: \$70 per linear foot
  - Street Lights: \$10,000 each

Using these assumed project costs, \$84,800 is available on each project for work elements such as sidewalks, curb ramps, and street lights. If all \$84,800 goes toward sidewalk improvements with curb and gutter, approximately 550 feet of sidewalk can be constructed. Separated sidewalks are desirable where possible to reduce costs for curb and gutter and leave budget available for other elements. To be conservative, recommended projects will not include more than 600 feet of sidewalk unless it is very likely that a separated sidewalk would be feasible (for example, if there is some existing separated sidewalk that could be extended).

## Final Rationale and Recommendations

The two highest ranking project areas were P (1) and A (2). These projects are included in the list of recommended projects below, but were not listed as highest priority for several reasons. For project P, there is an existing structure over the Jackson Drain that could potentially be used to provide a pathway between E Barbara Dr and the multi-use path, connecting the residents of the Fairview Terrace Estates mobile home park to the path and therefore to Chief Joseph Elementary. However, because no structural information is known at the time of this study, more information and structural design consultation would be needed to move forward with this project. Project area A is also included on the list but was shifted down in priority due to the size of the project, which will take multiple funding years to complete. The two projects ranked at the top of the list (G and F) will provide significant connections in the pedestrian network and can each be completed with one year of funds. The rest of the priority list was filled in largely following the analysis rankings, with some exceptions. Projects that are “stubs” or do not provide much connection value were moved to a lower priority on the list. Projects that are anticipated to have significant right-of-way or utility conflicts were also moved down the list. All the projects identified in the analysis will provide value to the community if constructed, but selecting the projects with both high value and low anticipated complexity and conflicts is recommended to maximize CDBG funds.

Based on the results of the analysis, Conсор has recommended the following 10 projects for future CDBG funding.

Table 4: Recommended Projects

Project Area Identifier	Priority	Project Description
G and F	1	Construct sidewalk (likely separated) on west side of NW 2 <sup>nd</sup> St from W Broadway Ave to Meridian Elementary with one to two infill lights. Construct sidewalk (likely with curb and gutter) on north side north side of W Broadway Ave from NW 3 <sup>rd</sup> St to NW 2 <sup>nd</sup> St.
F	2	Construct sidewalk (likely separated) on west side of NW 3 <sup>rd</sup> St from W Broadway Ave to W Pine Ave with one infill light per block. Construct sidewalk (likely with curb and gutter) on north side of W Broadway Ave from NW 4 <sup>th</sup> St to NW 3 <sup>rd</sup> St.
A (Phase 1)	3	Construct sidewalk (likely attached using existing curb and gutter) on west side of NW 7 <sup>th</sup> St from 1233 NW 7 <sup>th</sup> St to 1505 NW 7 <sup>th</sup> St, with lighting infill.
A (Phase 2)	4	Construct sidewalk (likely attached using existing curb and gutter) on west side of NW 7 <sup>th</sup> St from 1505 NW 7 <sup>th</sup> St to W Cherry Ln, with lighting infill.
A (Phase 3)	5	Construct sidewalk (likely attached using existing curb and gutter) on west side of NW 7 <sup>th</sup> St from W Carlton Ave 1233 NW 7 <sup>th</sup> St, with lighting infill.
D	6	Construct sidewalk (likely attached using existing curb and gutter) on north side of W Maple Ave from NW 4 <sup>th</sup> St to NW 2 <sup>nd</sup> St, with lighting infill.
N	7	Construct sidewalk (likely majority separated) on west side of E 4 <sup>th</sup> St from north of E Pine Ave to E Carlton Ave, with lighting infill. Continue lighting infill onto E Carlton Ave if budget allows.
J	8	Construct sidewalk (likely attached or new curb and gutter) on south side of E Badley Ave from NE 2 ½ St to east of E 3 <sup>rd</sup> St and on both sides of E 3 <sup>rd</sup> St from Badley to the south end. Infill lighting on E Badley Ave from NE 2 ½ St to Creekside Arbour Apartments.
K	9	If possible, construct sidewalk (separated) on south side (Cole Valley Christian School) of E Washington Ave from NE 2 ½ St to NE 4 <sup>th</sup> St. If not, construct sidewalk on north side – this is not preferred as there are many utility conflicts.
P	10	Construct a path over the Jackson Drain to connect E Barbara Dr and the multi-use path.







# Analysis~Identification of Future Sidewalk & Streetlight Projects (CDBG) Project Selection Criteria

**Date:** June 27, 2023

**Project:** Analysis~Identification of Future Sidewalk & Streetlight Projects (CDBG)

**To:** Crystal Campbell  
City of Meridian, Community Development Program Coordinator

**From:** Hannah Long, PE  
ConSOR, Project Engineer

**Reviewed By:** Jonathan Gellings, PE  
ConSOR, Principal Engineer

**Re:** Project Selection Criteria

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## Introduction

The purpose of this memorandum is to present the project selection criteria that have been developed to identify and prioritize project areas to receive future City of Meridian community development block grant (CDBG) funding.

## Project Selection Criteria Descriptions

The project selection criteria have been divided into three categories: pedestrian demand, pedestrian infrastructure, and safety. Each category contains multiple metrics which are defined in the following sections.

### Pedestrian Demand

Pedestrian demand is defined by the likelihood of pedestrians being present in a potential project area, or number of walk trips anticipated to occur in an area. This category contains four criteria: walkability index and proximity to schools, parks, and transit stops as defined below. Radial distances used in this analysis will be adjusted down to more closely represent actual walking distances which are confined to sidewalks and other paths.

**Walkability index:** This score will be informed by the National Walkability Index generated by the United States Environmental Protection Agency ([National Walkability Index \(arcgis.com\)](https://www.arcgis.com)). The National Walkability Index considers intersection density, proximity to transit stops, employment mix, and employment and household mix to produce a score based on the number of walk trips generated in certain areas defined by census block groups (higher walkability index corresponds to higher numbers of pedestrians anticipated in those areas).

**Proximity to school:** Distance (radially as the crow flies) from a school (public, school of choice, alternative, or charter). For public schools, radii shall be limited to the West Ada School District “Walk Zone” maps for each school.

**Proximity to park:** Distance (radially as the crow flies) from a public park.

**Proximity to transit stop:** Distance (radially as the crow flies) from a Valley Regional Transit (VRT) stop.

## Pedestrian Infrastructure

The pedestrian infrastructure category is comprised of two criteria: sidewalk continuity and ADA compliance, as defined below.

**Sidewalk continuity:** Whether there are gaps in the existing sidewalk, and how big the gaps are. Areas with no continuous sidewalk on either side of the road are highest priority, followed by areas with continuous sidewalk on only one side of the road. Areas with sidewalk gaps will be identified using Google Earth and confirmed with field visits.

**ADA compliance:** Whether a segment of sidewalk, including curb ramps, is ADA-compliant. Compliant sidewalks have a 4’ sidewalk width from back of curb free of clear width obstructions, run slopes of <5% where topography permits, a cross slope of 2% or less, and compliant ramps. Compliant ramps have at least a 4’x4’ level landing area (4’x5’ where turning space is constrained at the back of walk), a run slope of less than 8.33%, a cross slope of less than 2%, 10% maximum flared sides where provided, and a detectable warning surface for visually impaired users. ADA compliance will be evaluated during field visits.

## Safety

The safety category is comprised of three criteria: level of crime, crash history, and street lighting, as defined below.

**Level of crime:** Level of crime will be the measurement of the number of crimes reported near a project area, relative to other potential project areas. This information will come from a customized hot spot crime map from the Meridian Police Department’s analytical services manager showing crimes from the years 2021 and 2022.

**Crash history:** Number of vehicle/pedestrian crashes from 2018 through 2022 within the project area as shown on a map provided by the Meridian Police Department’s analytical services manager.

**Street lighting:** The adequacy of existing street lighting will be evaluated using light type and spacing, and whether based on those two factors the existing lighting is anticipated to meet applicable illumination guidelines in the IES RP-8-18 standard. Street light spacing, wattage, and type will be identified using the Streetlight Locator Map GIS page ([Streetlight Locator Map- For AI \(meridiantcity.org\)](#)) and confirmed during field visits.

## Other

In addition to the project areas identified by using the scoring criteria, areas that the City has previously identified as crucial improvement locations or areas where the City has received requests or feedback may be considered for projects. These special request areas will be evaluated using the scoring system but may be given higher priority at the City’s discretion based on historical or anecdotal information not captured in the scoring system.

Finally, if any of the potential project areas are already part of the ACHD Integrated Five Year Work Plan ([ACHD 2022-2026 Integrated Five Year Work Plan \(IFYWP\) \(arcgis.com\)](#)) and have overlapping improvements scheduled, they will be eliminated from CDBG project recommendations.

## Project Selection Criteria Scoring

Each criterion will be given a score from 0 to 4, with a 0 corresponding to lowest priority for a future CDBG project and a 4 corresponding to highest priority. Please see the attached rubric for descriptions of each score for all the criteria listed above. The table below shows the proposed weight to be applied to each score.

### Criteria Weighting Table

Criteria	Weight	Notes
Walkability Index	1	
Proximity to School	1	
Proximity to Park	0.7	Park proximity is given a lower weight as parks are anticipated to generate fewer walking trips than schools.
Proximity to Transit Stop	1	
Sidewalk Continuity	1.5	High importance for improving pedestrian network and initial project identification.
ADA Compliance	1.5	High importance for improving pedestrian network.
Level of Crime	1	
Crash History	1	
Street Lighting	1	



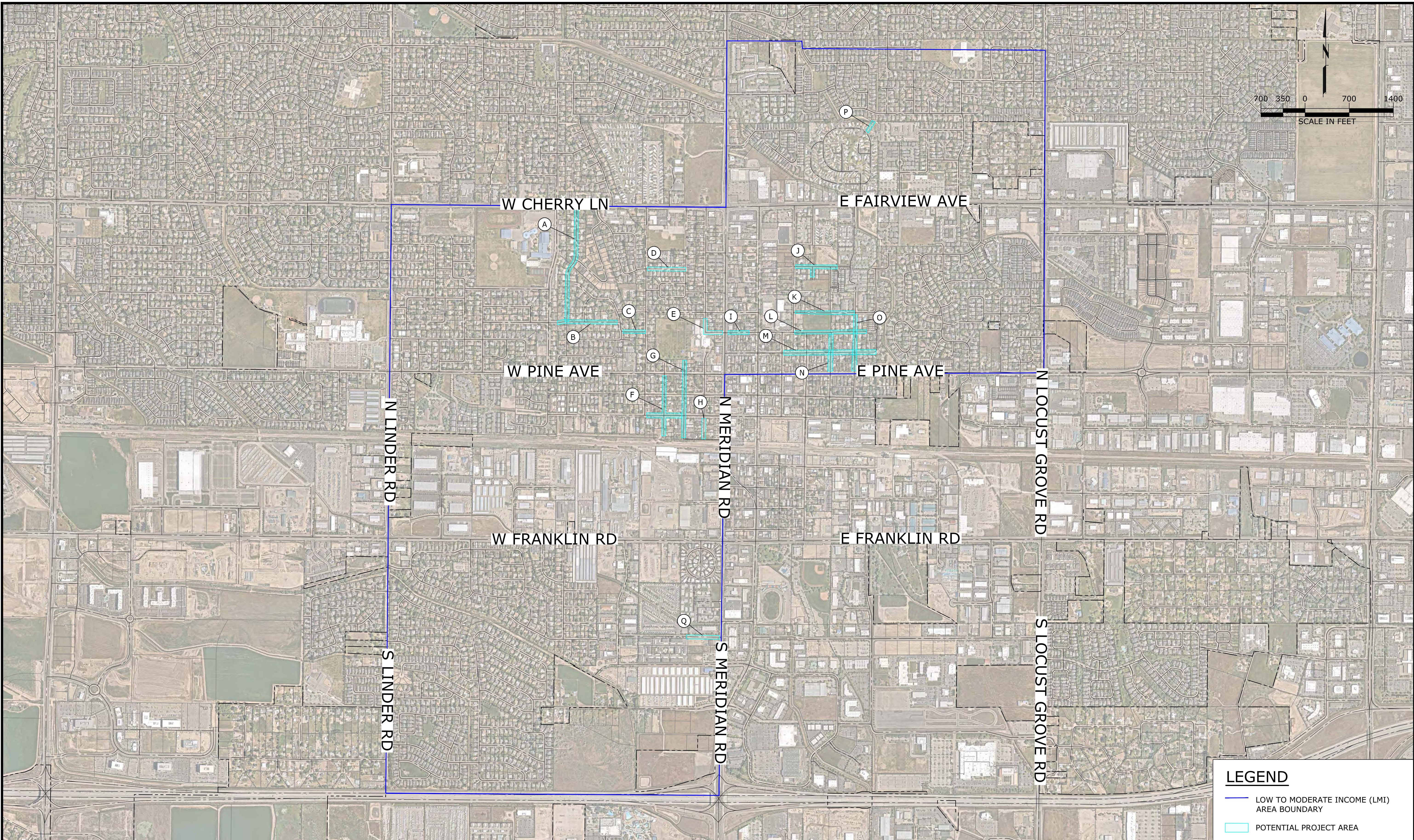
Analysis~Identification of Future Sidewalk & Streetlight Projects (CDBG)						
Project Selection Criteria Rubric						
Criteria	0	1	2	3	4	Notes
<b>Pedestrian Demand</b>						
Walkability index	N/A	EPA walkability index of 1-5.75 (least walkable)	EPA walkability index of 5.76-10.50 (below average walkable)	EPA walkability index of 10.51-15.25 (above average walkable)	EPA walkability index of 15.26-20 (most walkable)	Note that the EPA walkability index measures number of trips generated in an area, and thus a higher walkability index indicates that more pedestrians are likely to be present in that area. This makes areas with a higher walkability index an efficient location to target funding as improvements will benefit more people.
Proximity to School	Area is outside any school walk zone maps limits or over 0.80 miles from a school and walking is unlikely.	Area is inside a school walk zone map within 0.80 miles of a school (~20 minutes); walking is somewhat likely.	Area is inside a school walk zone map within 0.60 miles of a school (~15 minutes); walking is likely.	Area is inside a school walk zone map within 0.40 miles of a school (~10 minutes); walking is very likely.	Area is inside a school walk zone map within 0.20 miles of a school (~5 minutes); walking is extremely likely.	Public schools, schools of choice, alternative schools, and charter schools included. Radial distances represent actual walking distances of 1.0, 0.75, 0.50, and 0.25 miles.
Proximity to Park	Area is over 0.80 miles from a park and walking is unlikely.	Area is within 0.80 miles of a park (~20 minutes); walking is somewhat likely.	Area is within 0.60 miles of a park (~15 minutes); walking is likely.	Area is within 0.40 miles of a park (~10 minutes); walking is very likely.	Area is within 0.20 miles of a park (~5 minutes); walking is extremely likely.	Only public access parks included. Radial distances represent actual walking distances of 1.0, 0.75, 0.50, and 0.25 miles.
Proximity to Transit Stop	Area is over 0.80 miles from a VRT stop and walking is unlikely.	Area is within 0.80 miles of a VRT stop (~20 minutes); walking is somewhat likely.	Area is within 0.60 miles of a VRT stop (~15 minutes); walking is likely.	Area is within 0.40 miles of a VRT stop (~10 minutes); walking is very likely.	Area is within 0.20 miles of a VRT stop (~5 minutes); walking is extremely likely.	VRT stands for Valley Regional Transit, stop locations obtained from online VRT system map. Radial distances represent actual walking distances of 1.0, 0.75, 0.50, and 0.25 miles.
<b>Pedestrian Infrastructure</b>						
Sidewalk Continuity	Sidewalk is continuous on both sides of the road throughout area.	One side of the road has continuous sidewalk; some sidewalk is present on the other side but gaps exist.	One side of the road has continuous sidewalk. Little or no sidewalk is present on the other side.	Some sidewalk is present but there are gaps on both sides of the road. No sidewalk on either side of the road for lengths under 200 ft.	No sidewalk is present on either side of the road for lengths over 200 ft.	
ADA Compliance	All sidewalk and curb ramps in the area are ADA-compliant. Continuous sidewalks are provided on at least one side of the street.	A one-block sidewalk section has at least one instance of one of the following: cross slope over 2%, run slope over 5%, width less than 4', clear width obstructions, and non-compliant ramps.	A one-block sidewalk section has at least two instances of one of the following: cross slope over 2%, run slope over 5%, width less than 4', clear width obstructions, and non-compliant ramps.	A one-block sidewalk section has at least three instances of one of the following: cross slope over 2%, run slope over 5%, width less than 4', clear width obstructions, and non-compliant ramps.	A one-block sidewalk section has at least four instances of one of the following: cross slope over 2%, run slope over 5%, width less than 4', clear width obstructions, and non-compliant ramps. Alternately, no continuous sidewalk and/or no ramps are available.	If continuous sidewalk exists on one side of the road, ADA compliance is evaluated for the continuous sidewalk.
<b>Safety</b>						
Level of Crime	Lowest number of reported crimes relative to the other LMI areas.		Mid-range number of reported crimes relative to other LMI areas (medium blue on map).		Highest number of reported crimes relative to other LMI areas (light purple on map).	Based on crime "hot spot" style map documenting crime locations from 2018 to 2022, provided by City of Meridian police department.
Crash History	No documented vehicle/pedestrian collisions within the project area.	1 documented vehicle/pedestrian collisions within the project area.	2 documented vehicle/pedestrian collisions within the project area.	3 documented vehicle/pedestrian collisions within the project area.	4 documented vehicle/pedestrian collisions within the project area.	Based on map documenting vehicle/pedestrian collision locations from 2018 to 2022, provided by City of Meridian police department.

Street Lighting	Street light placement, spacing, and wattage indicate that the area meets IES RP-8-18 standards (approximate spacing between lights is under 175 ft for typical LED street lights).	Street lights are present at block corners and at midblock locations; some fixtures could be added at strategic locations to improve pedestrian visibility.	Street lights are present consistently at block corners/intersections; area could benefit from an average of one infill light per block or block distance equivalent.	Street lights are present in the area; they are sporadically placed and gaps of 450 to 600 ft exist. Area likely requires two lights added per block or block distance equivalent.	Very few street lights are present; gaps of over 600 ft exist. Area requires more than two lights added per block or block distance equivalent in places.	Block length is assumed to be 315 ft.
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**LEGEND**

- LOW TO MODERATE INCOME (LMI) AREA BOUNDARY
- POTENTIAL PROJECT AREA

NO.	DATE	BY	REVISION

NOTICE  
 0 1/2 1  
 IF THIS BAR DOES NOT MEASURE 1" THEN DRAWING IS NOT TO SCALE

HCL  
 DESIGNED  
 HCL  
 DRAWN  
 JMG  
 CHECKED

**EXHIBIT ONLY**  
 DO NOT USE FOR CONSTRUCTION  
 JUNE 2023  
 Consor  
 www.consorinc.com



**CITY OF MERIDIAN**  
**IDENTIFICATION OF**  
**FUTURE SIDEWALK &**  
**STREETLIGHT PROJECTS**  
**(CDBG)**

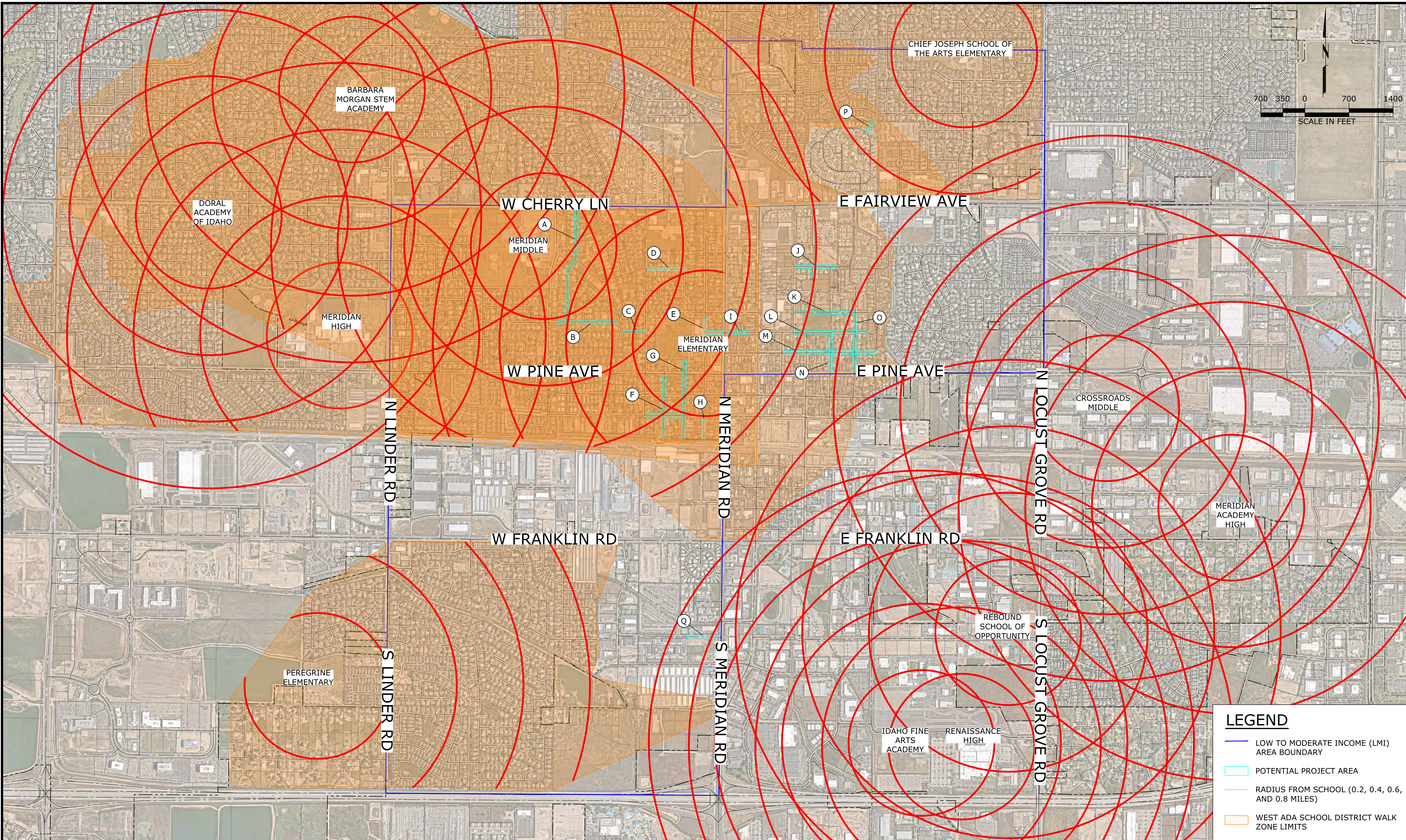
**POTENTIAL PROJECT AREAS**  
**OVERVIEW MAP**  
 PROJECT NO.: D231114ID SCALE: AS SHOWN DATE: JUNE 2023

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**LEGEND**

- LOW TO MODERATE INCOME (LMI) AREA BOUNDARY
- POTENTIAL PROJECT AREA
- RADIUS FROM SCHOOL (0.2, 0.4, 0.6, AND 0.8 MILES)
- WEST ADA SCHOOL DISTRICT WALK ZONE LIMITS

NO.	DATE	BY	REVISION

**NOTICE**  
 0 1/2 1  
 IF THIS BAR DOES NOT MEASURE 1" THEN DRAWING IS NOT TO SCALE

HCL  
 DESIGNED  
 HCL  
 DRAWN  
 JMG  
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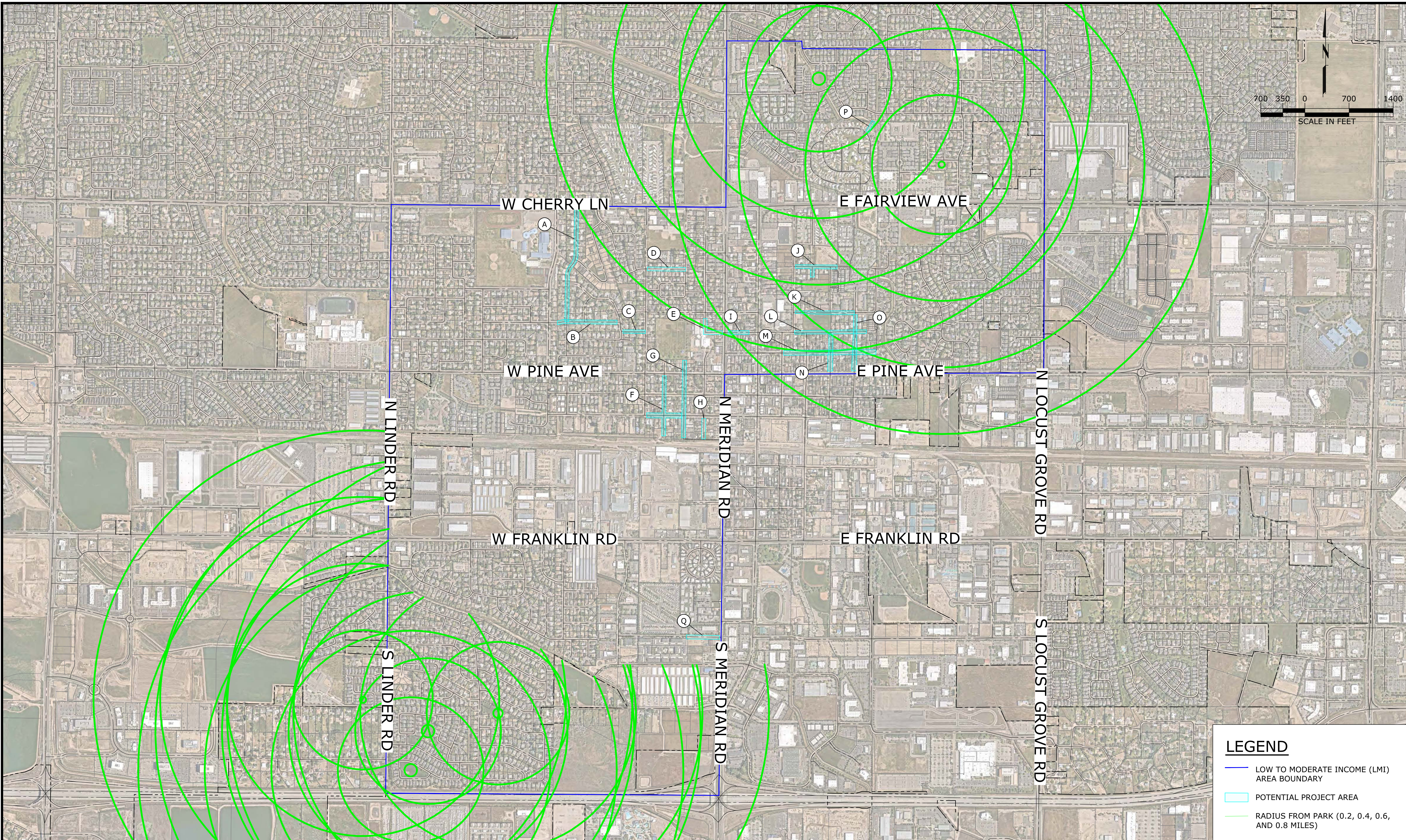


**CITY OF MERIDIAN**  
**IDENTIFICATION OF**  
**FUTURE SIDEWALK &**  
**STREETLIGHT PROJECTS**  
**(CDBG)**

**SCHOOL PROXIMITY**  
 PROJECT NO.: D231114ID SCALE: AS SHOWN DATE: JUNE 2023

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 D-2

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**LEGEND**

- LOW TO MODERATE INCOME (LMI) AREA BOUNDARY
- POTENTIAL PROJECT AREA
- RADIUS FROM PARK (0.2, 0.4, 0.6, AND 0.8 MILES)

NO.	DATE	BY	REVISION

NOTICE  
 0 1/2 1  
 IF THIS BAR DOES NOT MEASURE 1" THEN DRAWING IS NOT TO SCALE

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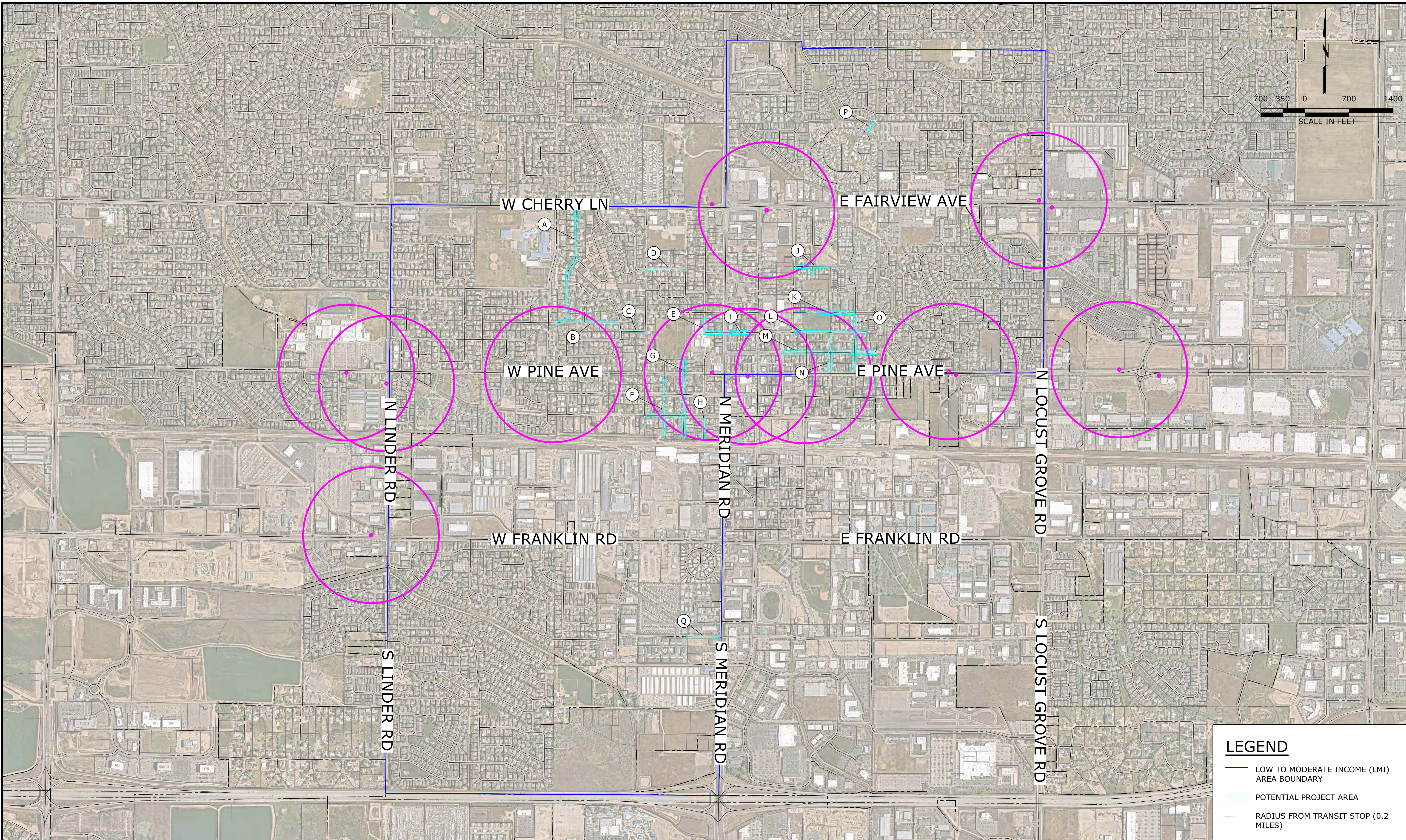
**CITY OF MERIDIAN**  
**IDENTIFICATION OF**  
**FUTURE SIDEWALK &**  
**STREETLIGHT PROJECTS**  
**(CDBG)**

**PARK PROXIMITY**

PROJECT NO.: D231114ID SCALE: AS SHOWN DATE: JUNE 2023

SHEET  
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**LEGEND**

- LOW TO MODERATE INCOME (LMI) AREA BOUNDARY
- POTENTIAL PROJECT AREA
- RADIUS FROM TRANSIT STOP (0.2 MILES)

NO.	DATE	BY	REVISION

NOTICE  
 0 1/2 1  
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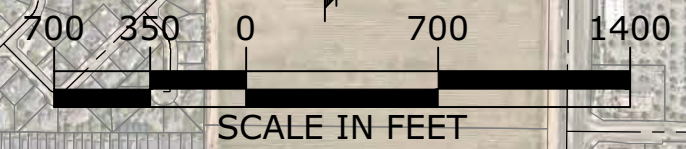
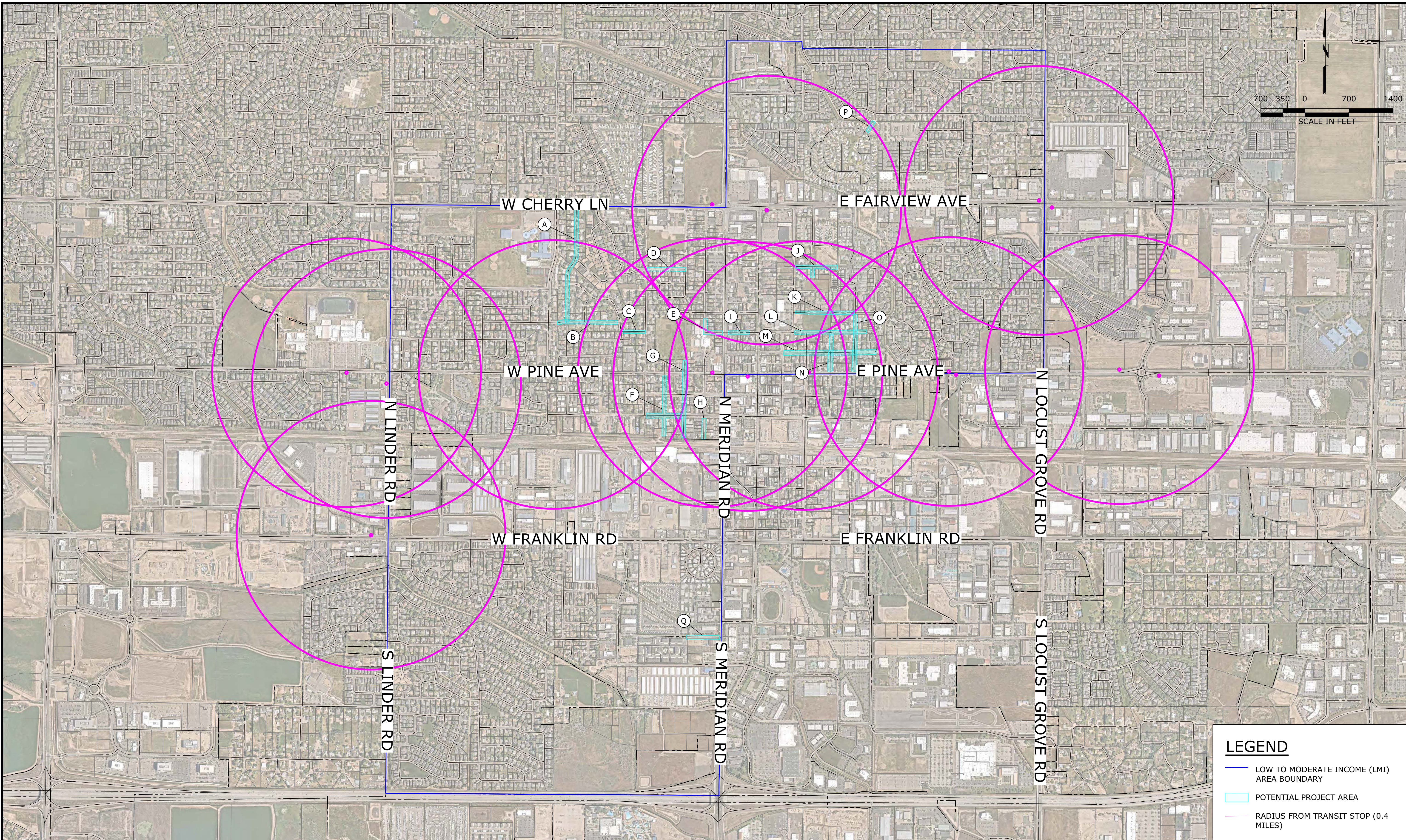
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**(CDBG)**

**TRANSIT STOP PROXIMITY**  
**0.2 MILES**

PROJECT NO.: D231114ID SCALE: AS SHOWN DATE: JUNE 2023

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**LEGEND**

- LOW TO MODERATE INCOME (LMI) AREA BOUNDARY
- POTENTIAL PROJECT AREA
- RADIUS FROM TRANSIT STOP (0.4 MILES)

NO.	DATE	BY	REVISION

NOTICE  
 0 1/2 1  
 IF THIS BAR DOES NOT MEASURE 1" THEN DRAWING IS NOT TO SCALE

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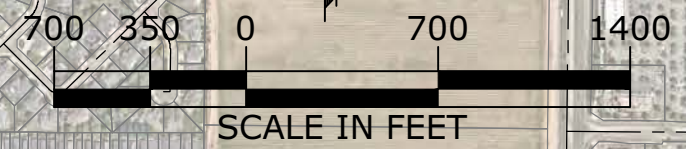
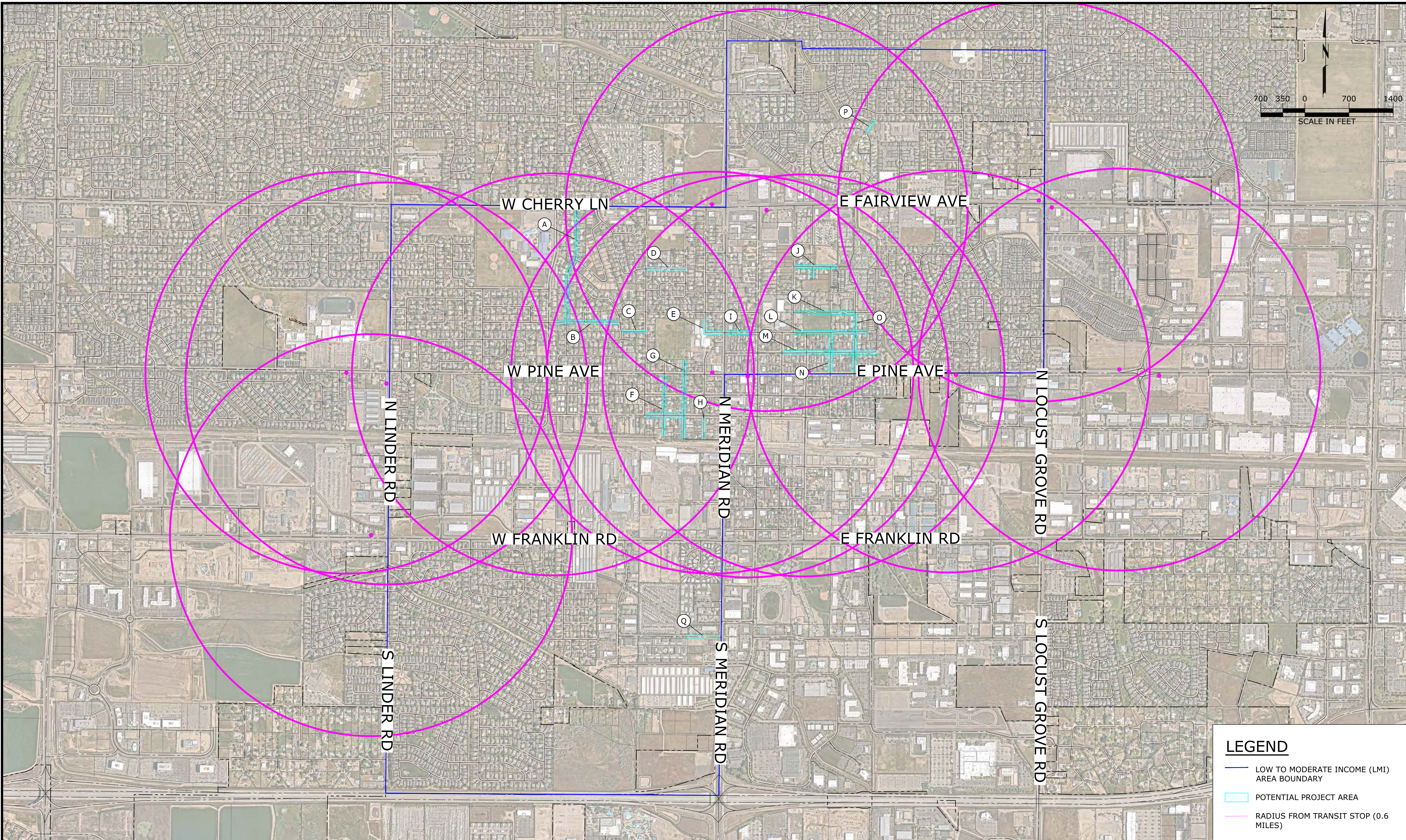
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**STREETLIGHT PROJECTS**  
**(CDBG)**

**TRANSIT STOP PROXIMITY**  
**0.4 MILES**

PROJECT NO.: D231114ID SCALE: AS SHOWN DATE: JUNE 2023

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**LEGEND**

- LOW TO MODERATE INCOME (LMI) AREA BOUNDARY
- POTENTIAL PROJECT AREA
- RADIUS FROM TRANSIT STOP (0.6 MILES)

NO.	DATE	BY	REVISION

NOTICE  
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**STREETLIGHT PROJECTS**  
**(CDBG)**

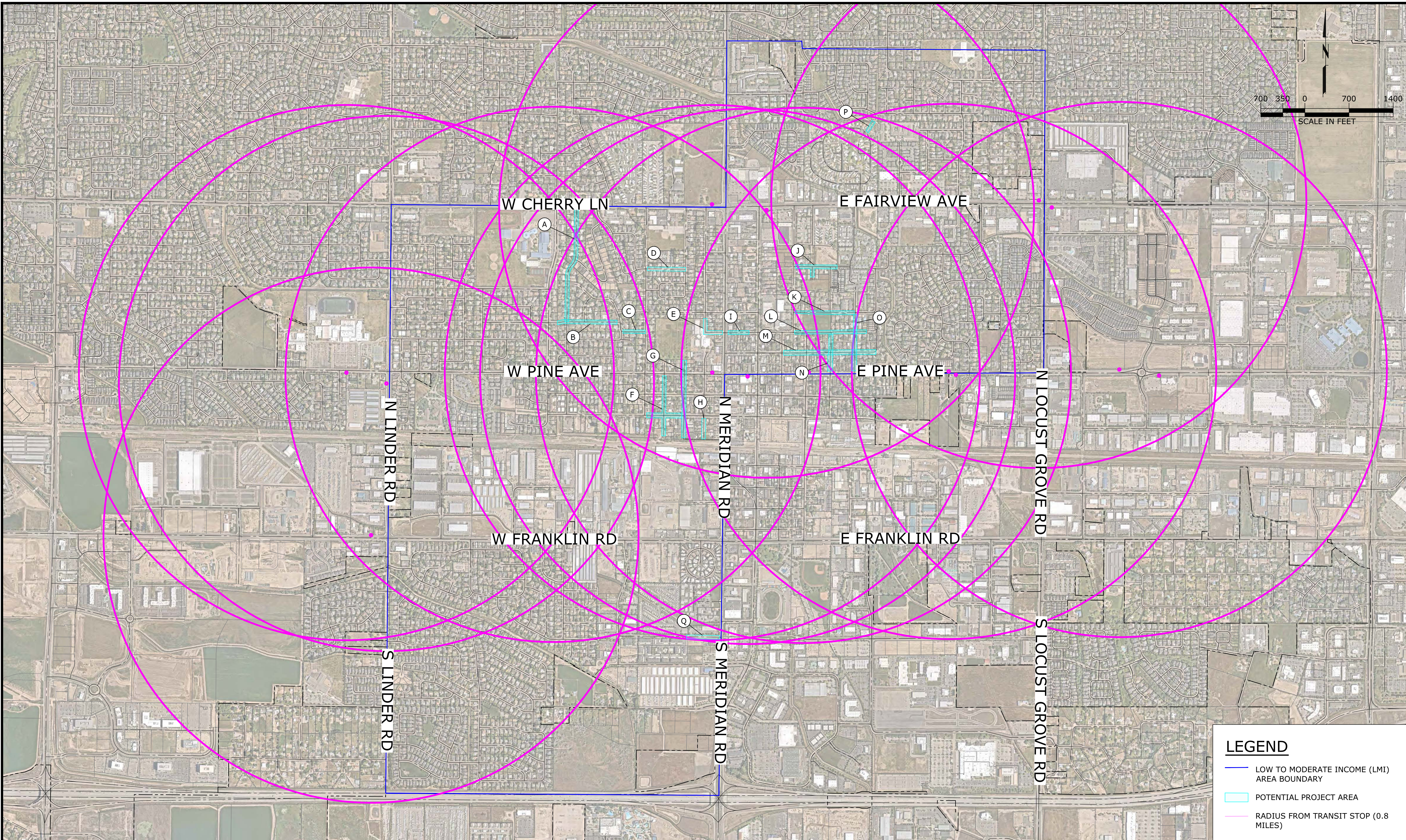
**TRANSIT STOP PROXIMITY**  
**0.6 MILES**

PROJECT NO.: D231114ID SCALE: AS SHOWN DATE: JUNE 2023

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**LEGEND**

- LOW TO MODERATE INCOME (LMI) AREA BOUNDARY
- POTENTIAL PROJECT AREA
- RADIUS FROM TRANSIT STOP (0.8 MILES)

NO.	DATE	BY	REVISION

NOTICE  
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**TRANSIT STOP PROXIMITY**  
**0.8 MILES**

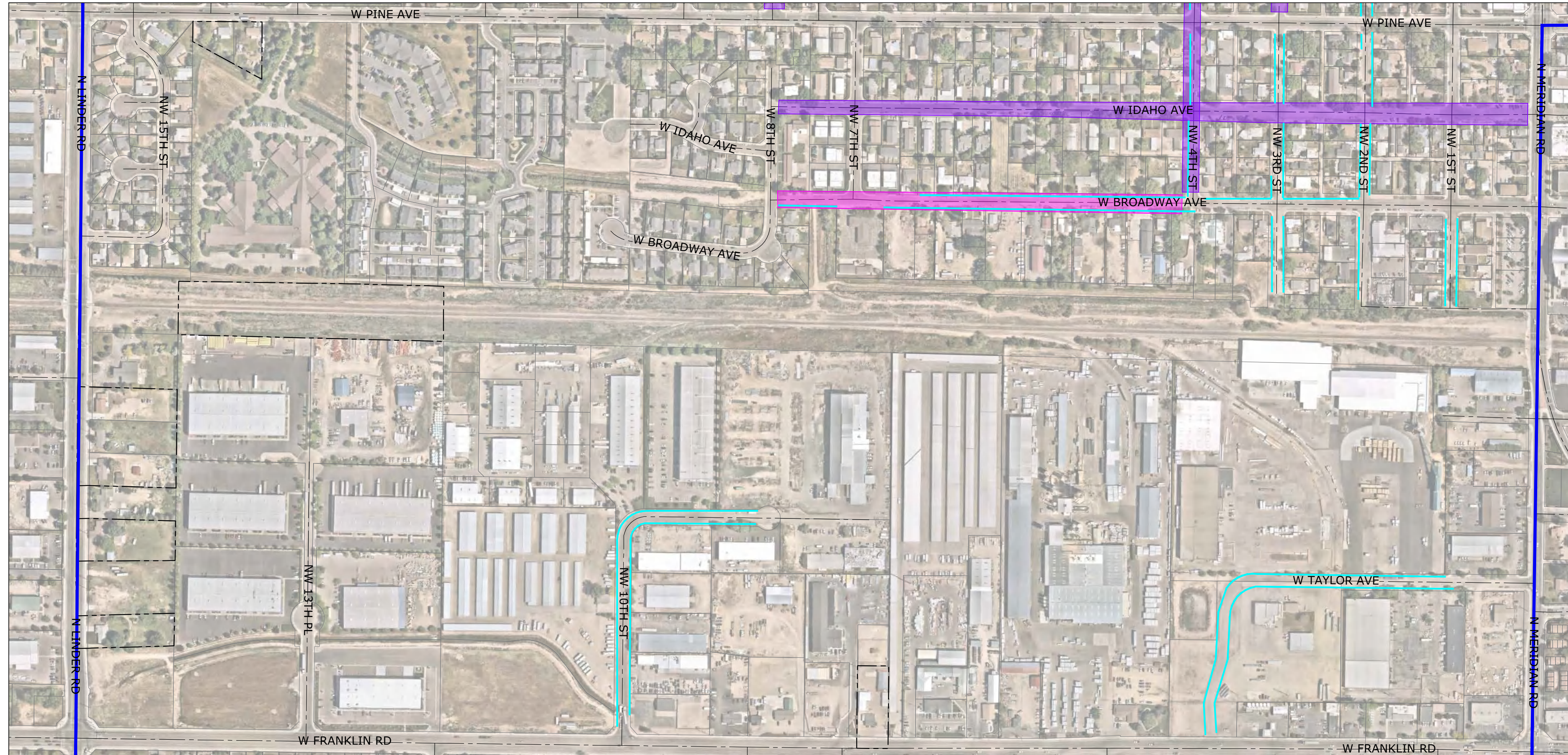
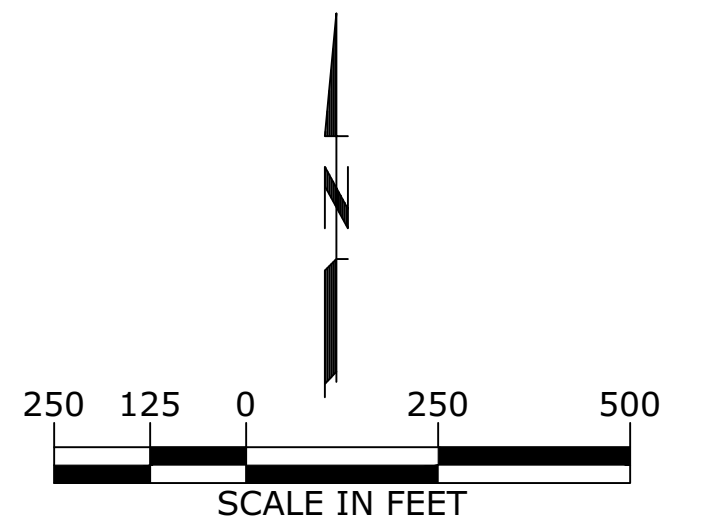
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**LEGEND**

- LOW TO MODERATE INCOME (LMI) AREA BOUNDARY
- SIDEWALK GAP
- ACHD IFYWP PROJECT - SCHEDULED PEDESTRIAN IMPROVEMENTS
- ACHD IFYWP PROJECT - POSSIBLE PEDESTRIAN IMPROVEMENTS

NO.	DATE	BY	REVISION

NOTICE  
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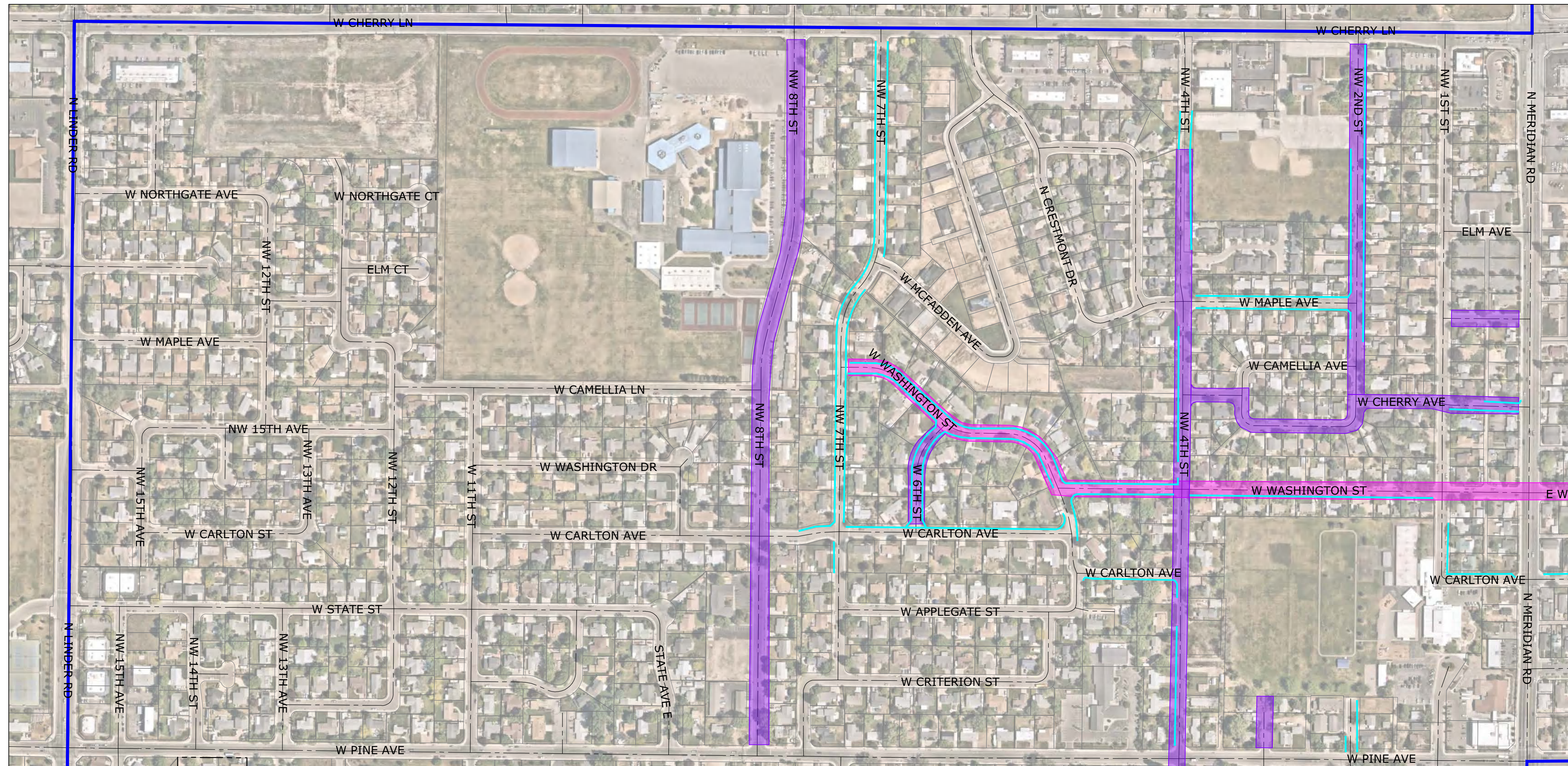
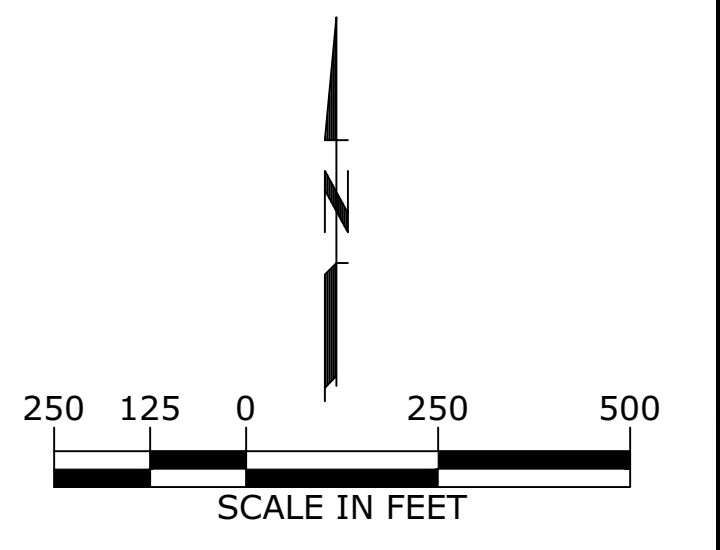


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 (CDBG)**

<b>SIDEWALK GAP MAP 2</b>			
PROJECT NO.:	D231114ID	SCALE:	AS SHOWN
DATE:	JUNE 2023		

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**LEGEND**

- LOW TO MODERATE INCOME (LMI) AREA BOUNDARY
- SIDEWALK GAP
- ACHD IFYWP PROJECT - SCHEDULED PEDESTRIAN IMPROVEMENTS
- ACHD IFYWP PROJECT - POSSIBLE PEDESTRIAN IMPROVEMENTS

NO.	DATE	BY	REVISION

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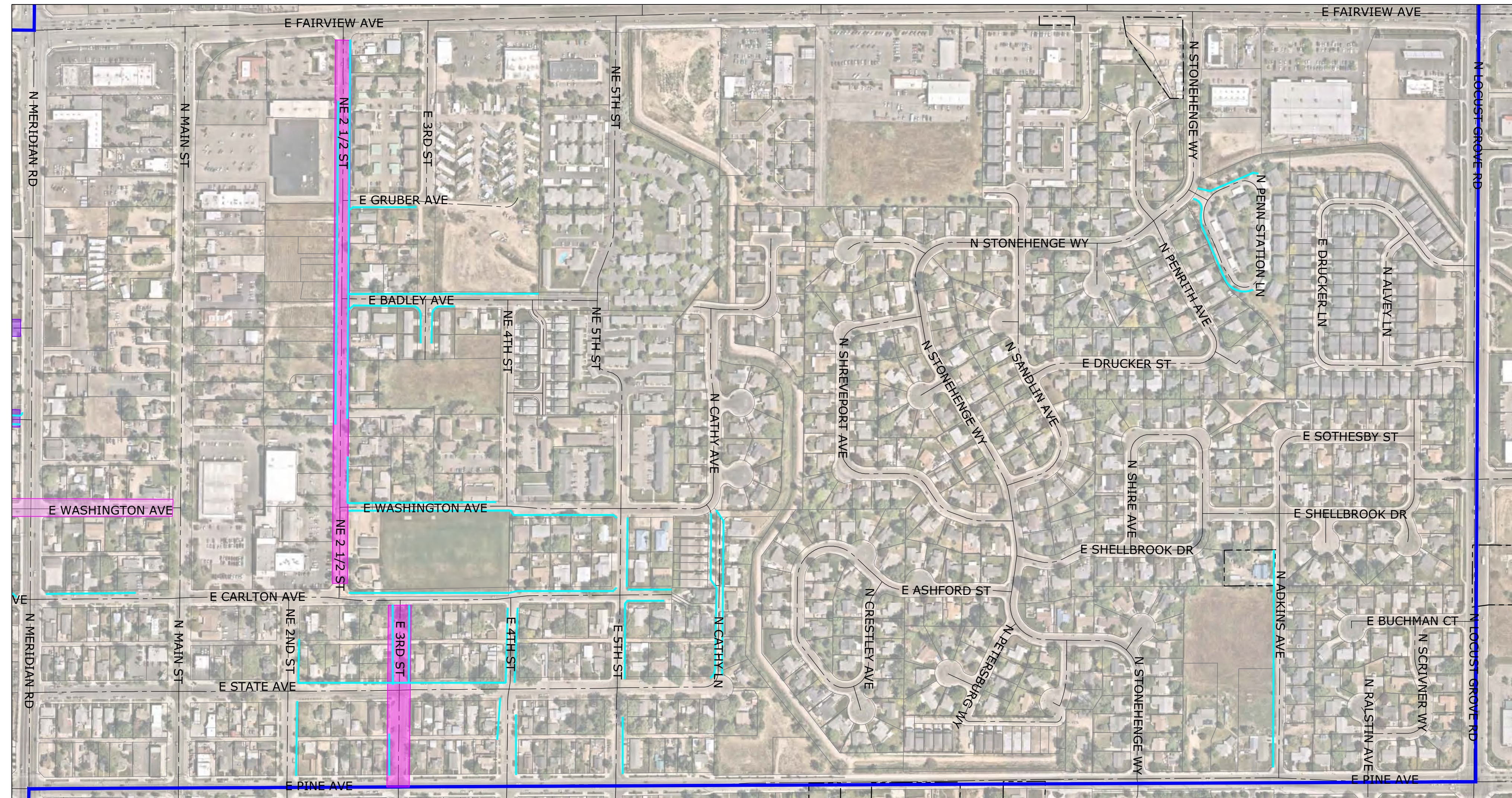
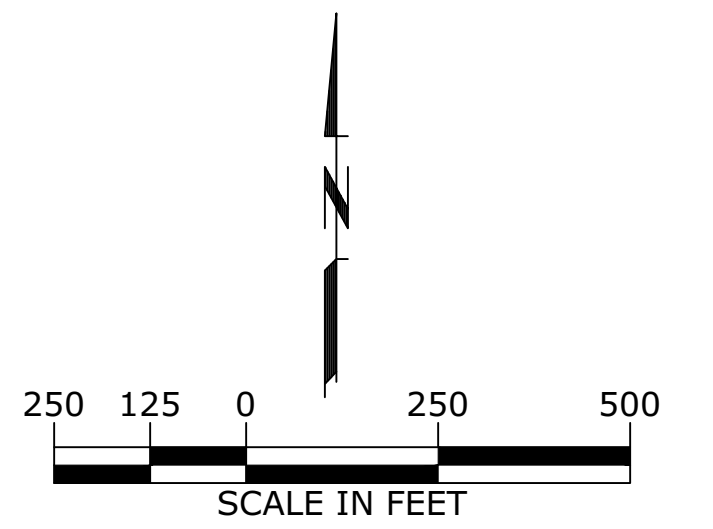


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**SIDEWALK GAP MAP**  
**3**  
 PROJECT NO.: D231114ID SCALE: AS SHOWN DATE: JUNE 2023

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**E-1C**

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**LEGEND**

- LOW TO MODERATE INCOME (LMI) AREA BOUNDARY
- SIDEWALK GAP
- ACHD IFYWP PROJECT - SCHEDULED PEDESTRIAN IMPROVEMENTS
- ACHD IFYWP PROJECT - POSSIBLE PEDESTRIAN IMPROVEMENTS

NO.	DATE	BY	REVISION

NOTICE  
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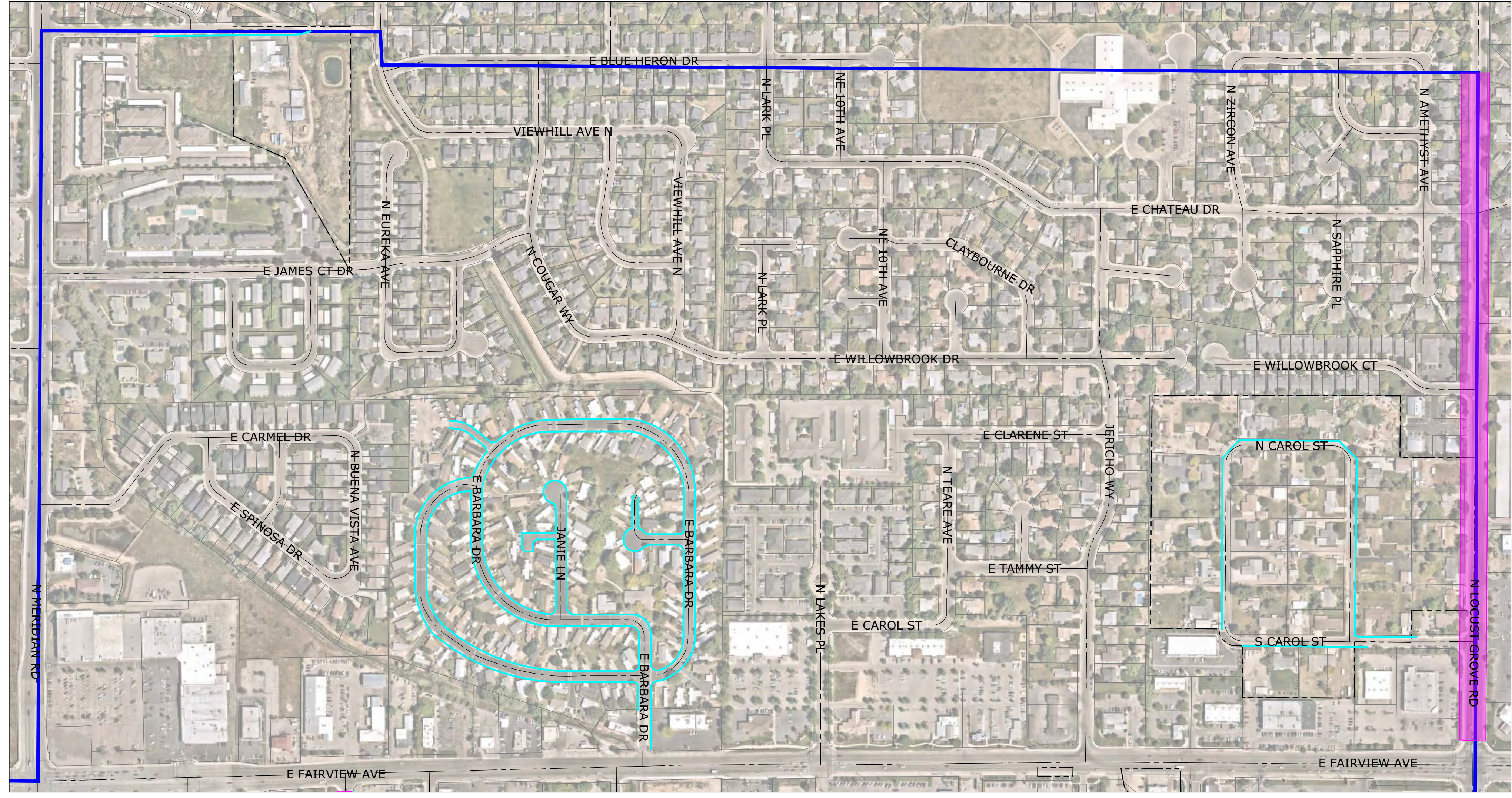
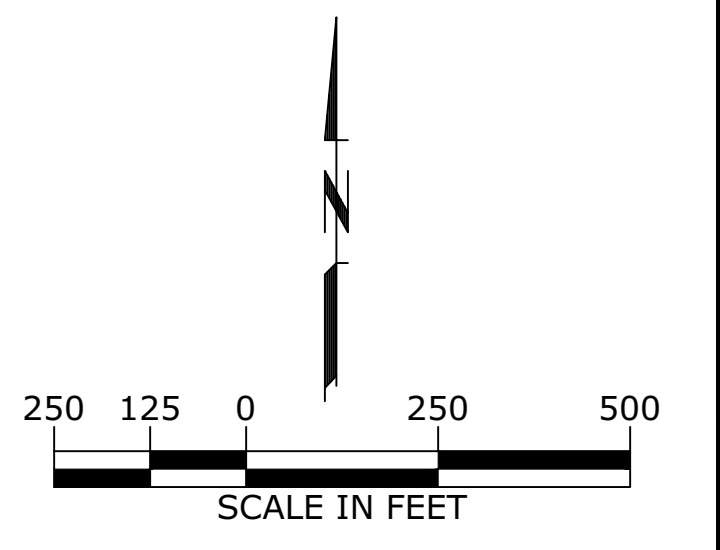


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**SIDEWALK GAP MAP**  
**4**  
 PROJECT NO.: D231114ID SCALE: AS SHOWN DATE: JUNE 2023

SHEET  
**E-1D**

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**LEGEND**

- LOW TO MODERATE INCOME (LMI) AREA BOUNDARY
- SIDEWALK GAP
- ACHD IFYWP PROJECT - SCHEDULED PEDESTRIAN IMPROVEMENTS
- ACHD IFYWP PROJECT - POSSIBLE PEDESTRIAN IMPROVEMENTS

NO.	DATE	BY	REVISION

NOTICE  
 0 1/2 1  
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**SIDEWALK GAP MAP**  
**5**  
 PROJECT NO.: D231114ID SCALE: AS SHOWN DATE: JUNE 2023

SHEET  
**E-1E**



## ADA Compliance

Project Area	ADA Compliance Score	Field Observations
A	4	No continuous sidewalk on either side of the road.
B	0	Continuous sidewalk on south side of the road. ADA ramps present at corners. All sidewalk and ramps are ADA compliant.
C	0	Continuous sidewalk on north side of the road. ADA ramps present at corners. All sidewalk and ramps are ADA compliant.
D	4	No continuous sidewalk on either side of the road.
E	2	Continuous sidewalk on south side of Carlton and west side of 1 <sup>st</sup> St. Two instances of run slopes well over 5% on 1 <sup>st</sup> St.
F	4	No continuous sidewalk on either side of the road.
G	4	No continuous sidewalk on either side of the road.
H	4	No continuous sidewalk on either side of the road.
I	0	Continuous sidewalk on south side of Carlton Ave. ADA ramps present at corners. All sidewalk and ramps are ADA compliant.
J	4	No continuous sidewalk on either side of the road.
K	4	No continuous sidewalk on either side of the road.
L	4	No continuous sidewalk on either side of the road.
M	0	Continuous sidewalk on south side of State Ave. ADA ramps present at corners. All sidewalk and ramps are ADA compliant.
N	4	No continuous sidewalk on either side of the road.
O	0	Continuous sidewalk on west side of 5 <sup>th</sup> St. ADA ramps present at corners. All sidewalk and ramps are ADA compliant.
P	4	No continuous sidewalk at this proposed drain crossing.
Q	3	Continuous sidewalk on north side of Pennwood. Two clear width obstructions (mailboxes) and noncompliant ramps at entrance to KOA RV park. Compliant ramp at Meridian Rd intersection.

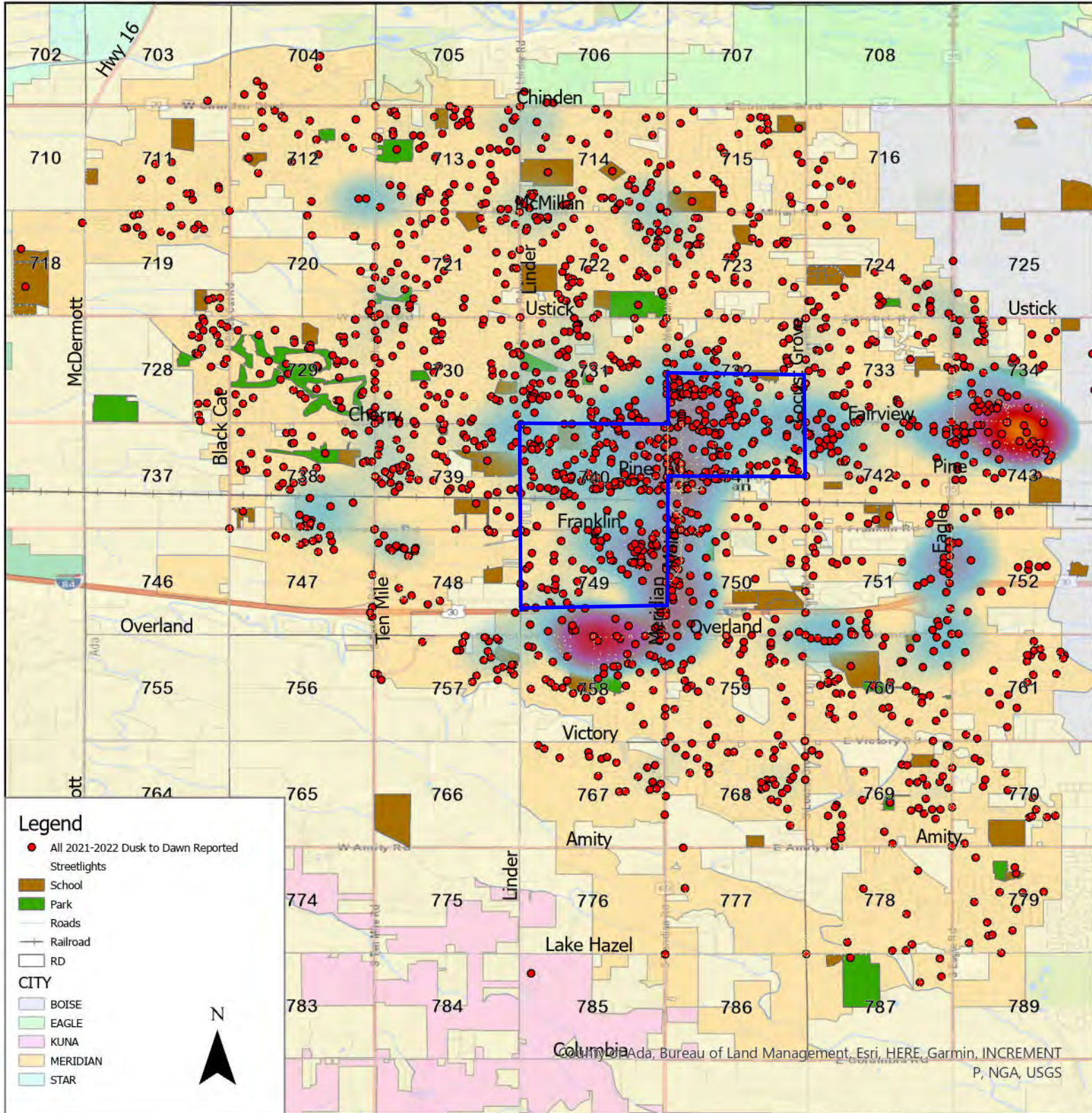






# Meridian Reported Crimes - Street Lights

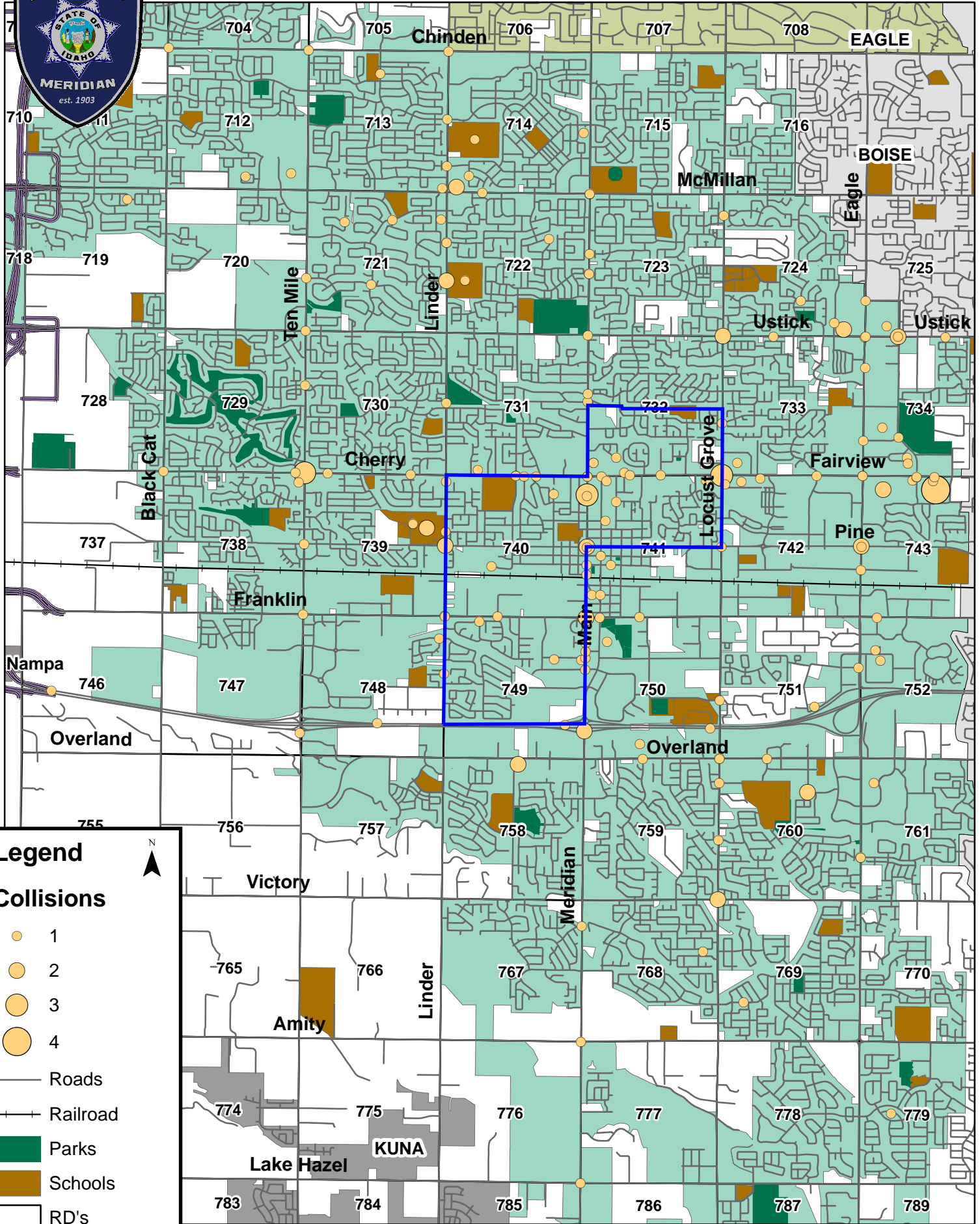
## 2021-2022



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NOTE: This map is displaying crimes reported to the state per NIBRS policy (EXCEPT for Vandalism incidents which all incidents are included for the purpose of this map). Street light in Meridian turn on between "dusk" and "dawn". The crimes displayed are those that occurred through an average of the monthly dusk to dawn time. Not all crimes will be affected by having or not having street lights and may have occurred within a structure away from such lights.

# Vehicle VS Pedestrian 2018-2022



**Legend**

**Collisions**

- 1
- 2
- 3
- 4

- Roads
- Railroad
- Parks
- Schools
- RD's
- Future Hwy 16

## Street Lighting

Project Area	Street Lighting Score	Comments
A	4	Some street lights are present but somewhat sporadically placed. Maximum gap between lights is approximately 605'.
B	4	Some street lights are present but somewhat sporadically placed. Maximum gap between lights is approximately 700'.
C	2	Street lights placed at intersection corners. Maximum gap between lights is approximately 435'.
D	2	Street lights are placed at intersection corners with one midblock location; however, block is longer than typical and could benefit from additional midblock lighting. Maximum gap between lights is approximately 330'.
E	2	Street lights are placed at intersection corners. Maximum gap between lights is approximately 330'.
F	2	Street lights are placed at intersection corners. Maximum gap between lights is approximately 380'.
G	2	Street lights are placed at intersection corners. Maximum gap between lights is approximately 370'.
H	2	Street lights are placed at intersection corners. Maximum gap between lights is approximately 360'.
I	3	Street lights are placed at intersection corners. Maximum gap between lights is approximately 470'.
J	2	Street lights are placed at intersection corners. Maximum gap between lights is approximately 350'.
K	2	Street lights are placed at intersection corners with one midblock location; however, block is longer than typical and could benefit from additional midblock lighting. Maximum gap between lights is approximately 360'.
L	2	Street lights are placed at intersection corners. Maximum gap between lights is approximately 380'.
M	3	Street lights are placed at intersection corners. Maximum gap between lights is approximately 450'.
N	2	Street lights are placed at intersection corners. Maximum gap between lights is approximately 320'.
O	2	Street lights are placed at intersection corners. Maximum gap between lights is approximately 370'.
P	4	No street lighting is present in the vicinity of this crossing location.
Q	3	Some street lights are present but are sporadically placed. Maximum gap between lights is approximately 550'.

Location A.....	36
Location B.....	45
Location C.....	49
Location D.....	52
Location E.....	55
Location F.....	58
Location G.....	65
Location H.....	74
Location I.....	76
Location J.....	78
Location K.....	82
Location L.....	86
Location M.....	91
Location N.....	96
Location O.....	101
Location P.....	106
Location Q.....	108

## Location A Field Notes

- **Sidewalk Continuity**
  - No sidewalk on west side
  - No sidewalk on east side except a little near Cedarburg Dr (newer development)
- **Ada Compliance**
  - Existing sidewalk near Cedarburg Dr is newer and ADA compliant
  - Existing ramps at Cedarburg Dr, pedestrian crossing, and SW corner of 7<sup>th</sup>/Cherry are compliant
  - Existing ramp at SE corner of 7<sup>th</sup>/Cherry is non-compliant – run slope over 12%
- **Street Lighting**
  - Light at NE corner of 7<sup>th</sup> and Carlton
  - Light at NE corner of 7<sup>th</sup> and Washington
  - Light at west side of pedestrian crossing
  - Light at NE corner of Cedarburg
  - Light on east side between Cedarburg and Cherry
  - Light at SE corner of 7<sup>th</sup> and Cherry
- **Other**
  - There is a path cut through near McFadden that provides pedestrian access from 7<sup>th</sup> St to 8<sup>th</sup> St. There is an existing signed pedestrian crossing on 7<sup>th</sup> St at this location.
  - Existing curb and rolled gutter along all of 7<sup>th</sup>, both sides.
  - Constructing sidewalk on west side may result in slightly less property restoration. Utility conflicts are similar on both sides.

# LOCATION A PHOTOS



# LOCATION A PHOTOS

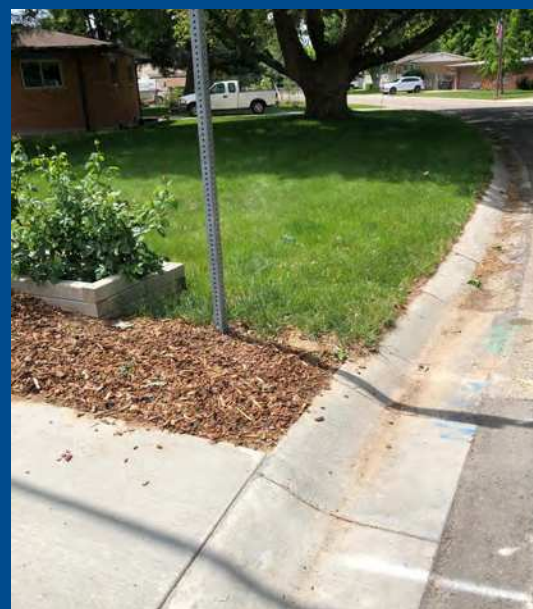
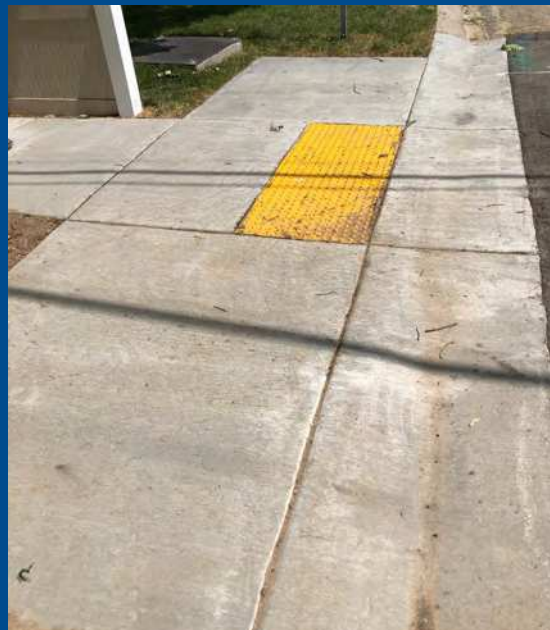




# LOCATION A PHOTOS



# LOCATION A PHOTOS



# LOCATION A PHOTOS



# LOCATION A PHOTOS



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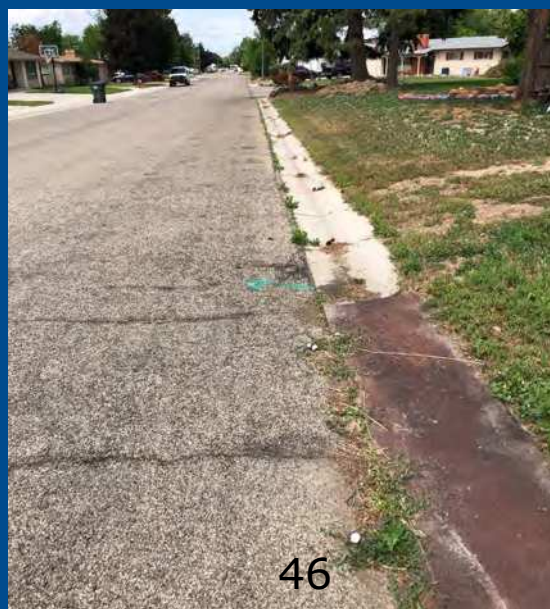




## Location B Field Notes

- **Sidewalk Continuity**
  - Existing continuous sidewalk on south side from 8<sup>th</sup> to 5<sup>th</sup>
  - No sidewalk on north side (sidewalk starts about 130ft west of 7<sup>th</sup>)
- **Ada Compliance**
  - Existing sidewalk on south side is ADA compliant
  - Existing ramps at SW and SE corner of Carlton/5<sup>th</sup>, SW corner of Carlton/6<sup>th</sup>, SW and SE corners of Carlton/7<sup>th</sup>, and all corners of Carlton/8<sup>th</sup> are compliant
- **Street Lighting**
  - Light at NW corner of Carlton/6<sup>th</sup>
  - Light at NE corner of Carton/7<sup>th</sup>
  - Light at SW corner of Carlton/8<sup>th</sup>
- **Other**
  - Existing curb and rolled gutter along all of Carlton, both sides.
  - Possible tree removal, several properties with hardscaping (rocks, lumber) and plantings to remove/relocate.

# LOCATION B PHOTOS





# LOCATION B PHOTOS



# LOCATION B PHOTOS



## Location C Field Notes

- **Sidewalk Continuity**
  - Existing continuous sidewalk on north side from 5<sup>th</sup> to 4<sup>th</sup>
  - No sidewalk on south side
- **Ada Compliance**
  - Existing sidewalk on north side is ADA compliant
  - Existing ramps at NW and NE corners of Carlton/4<sup>th</sup>, SW corner of Carlton/5<sup>th</sup> are compliant
  - Existing ramp at SE corner of Carlton/5<sup>th</sup> is non-compliant (no truncated domes, level landing slope of ~8%)
- **Street Lighting**
  - Light at SW corner of Carlton/5<sup>th</sup>
  - Light at NE corner of Carton/4<sup>th</sup>
- **Other**
  - Existing curb and rolled gutter along all of Carlton, both sides.
  - Possible tree conflict, extensive hardscaping conflicts including concrete stairs, brick wall, pavers, landscape lighting, chain link fence, and paver retaining wall.

# LOCATION C PHOTOS



# LOCATION C PHOTOS



## Location D Field Notes

- **Sidewalk Continuity**
  - No sidewalk on either side of Maple
- **Ada Compliance**
  - Existing ramps at SW and NW corners of Maple and 4<sup>th</sup> are compliant
- **Street Lighting**
  - Light at SW corner of Maple/4<sup>th</sup>
  - Light at midblock, south side
  - Light at SE corner of Maple/2<sup>nd</sup>
- **Other**
  - Existing curb and rolled gutter along all of Maple, both sides.
  - Several conflicts on both sides of the road with mailboxes, plants, and hardscaping (fences, short retaining walls). There appear to be fewer and less complicated conflicts on the north side.

# LOCATION D PHOTOS



# LOCATION D PHOTOS





## Location E Field Notes

- **Sidewalk Continuity**
  - Sidewalk on south side of Carlton and west side of 1<sup>st</sup>
  - No sidewalk on north side of Carlton
  - Sidewalk on east side of 1<sup>st</sup> ends ~100 ft south of Washington
- **Ada Compliance**
  - Existing ramps at SE and NE corners of Carlton and Meridian Rd are compliant
  - Existing ramps at school driveways on south side of Carlton are compliant
  - Existing sidewalk has two spots where run slope is well over 5%.
- **Street Lighting**
  - Light at NE corner of Carlton/1<sup>st</sup>
  - Intersection lighting at Carlton/Meridian and 1<sup>st</sup>/Washington
- **Other**
  - Existing curb and rolled gutter along all of Carlton, both sides.
  - Minor landscaping conflicts
  - One utility pole conflict at corner of Carlton/1<sup>st</sup> unless sidewalk can be separated

# LOCATION E PHOTOS



# LOCATION E PHOTOS



## Location F Field Notes

- **Sidewalk Continuity**
  - No sidewalk on east side of 3<sup>rd</sup>
  - Segment of separated sidewalk on west side of 3<sup>rd</sup> from Idaho Ave to 200 ft south
  - Existing sidewalk on south side of Broadway
  - No sidewalk on north side of Broadway
- **Ada Compliance**
  - Existing ramps at NE, NW, SE and SW corners of 3<sup>rd</sup>/Pine, SW/SE corners of 3<sup>rd</sup>/Broadway, NE/SE corners of 4<sup>th</sup>/Broadway, and SW/SE corners of Broadway/2<sup>nd</sup> are compliant
  - Existing ramps at NE and NW corners of Idaho would not be compliant with new sidewalk – they are single-directional
  - Existing sidewalk on west side of 3<sup>rd</sup> appears ADA-compliant but several panels are in poor condition with cracking
  - Existing sidewalk on Broadway appears to be ADA-compliant
- **Street Lighting**
  - Light at NE corner of 3<sup>rd</sup>/Pine
  - Light at SE corner of 3<sup>rd</sup>/Idaho
  - Light at SW corner of 3<sup>rd</sup>/Broadway
  - Light at SW end of 3<sup>rd</sup>
  - Light at SE corner of Broadway/4<sup>th</sup>
  - Light at SW corner of Broadway/2<sup>nd</sup>
- **Other**
  - No existing curb and gutter, likely that a separated sidewalk is possible on 3<sup>rd</sup> but likely Broadway sidewalk would need curb and gutter
  - Minor landscaping conflicts
  - Likely drainage inlet relocations would be necessary

# LOCATION F PHOTOS



# LOCATION F PHOTOS



# LOCATION F PHOTOS



# LOCATION F PHOTOS





# LOCATION F PHOTOS



# LOCATION F PHOTOS





## Location G Field Notes

- **Sidewalk Continuity**
  - Sidewalk on east side of 2<sup>nd</sup> from Broadway to south end
  - Sidewalk on west side of 2<sup>nd</sup> from Meridian Elementary school to 120 ft south
  - Sidewalk on west side of 2<sup>nd</sup> from Idaho to 200 ft north
- **Ada Compliance**
  - Existing ramps at NE, NW, SE and SW corners of 2<sup>nd</sup>/Pine and SW/SE corners of 2<sup>nd</sup>/Broadway are compliant
  - Existing ramps at SE and SW corners of Idaho would not be compliant with new sidewalk – they are single-directional
  - Existing sidewalk on 2<sup>nd</sup> is ADA-compliant
- **Street Lighting**
  - Light at NE corner of 2<sup>nd</sup>/Pine
  - Light at SW corner of 2<sup>nd</sup>/Idaho
  - Light at SW corner of 2<sup>nd</sup>/Broadway
- **Other**
  - No existing curb and gutter, likely that a separated sidewalk is possible
  - Minor landscaping conflicts
  - Likely drainage inlet relocations would be necessary
  - Likely utility pole conflicts on west side of 2<sup>nd</sup>/Idaho

# LOCATION G PHOTOS



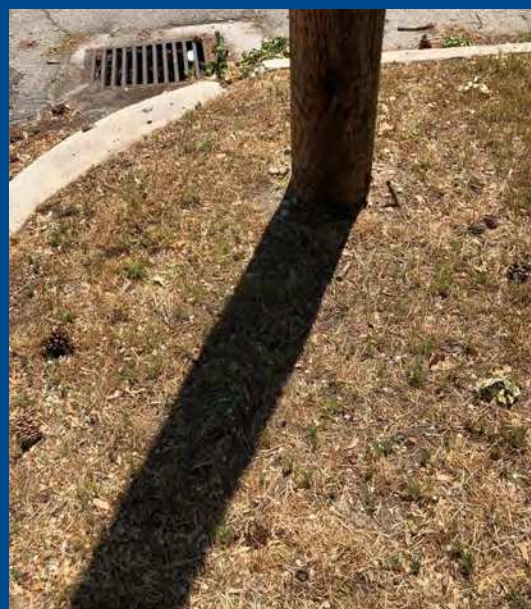
# LOCATION G PHOTOS



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# LOCATION G PHOTOS



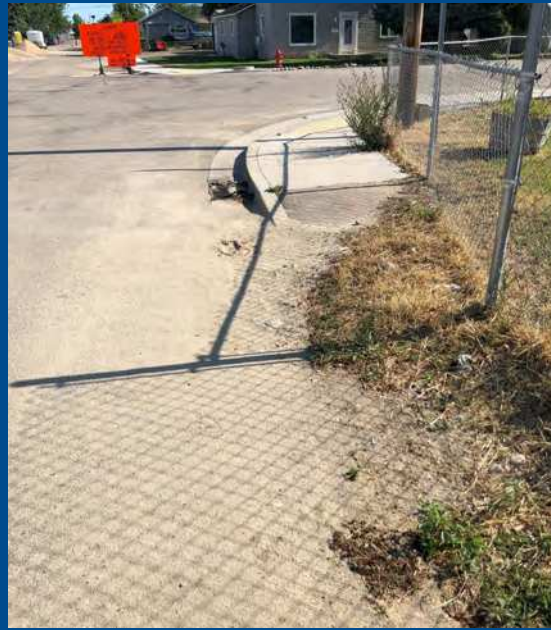
# LOCATION G PHOTOS



## Location H Field Notes

- **Sidewalk Continuity**
  - No sidewalk on either side of 1<sup>st</sup>
- **Ada Compliance**
  - Existing ramps at SE and SW corners of 1<sup>st</sup>/Broadway are compliant
- **Street Lighting**
  - Light at SE corner of 1<sup>st</sup>/Broadway
- **Other**
  - No existing curb and gutter, likely that a separated sidewalk is possible
  - Few to no utility or landscaping conflicts

# LOCATION H PHOTOS

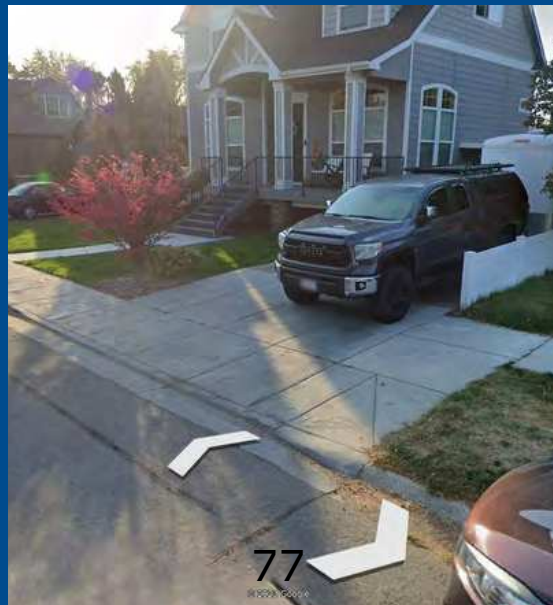




## Location I Field Notes

- **Sidewalk Continuity**
  - Existing sidewalk on south side of Carlton
  - Existing sidewalk on north side of Carlton from Main St to ~120 ft west
- **Ada Compliance**
  - Existing ramps at SE and NE corners of Carlton/Meridian and SW, NW, NE, and SE corners of Carlton/Main are compliant
  - Existing sidewalk on Carlton is ADA compliant
- **Street Lighting**
  - Street light at SE corner of Carlton/Meridian (for pedestrian crossing signal)
  - Pedestrian scale light at NE corner of Carlton/Meridian
  - Street lights at NE and SW corners of Carlton/Main
  - Pedestrian scale lights at NW and SE corners of Carlton/Main
- **Other**
  - Existing curb and gutter everywhere on Carlton
  - Minor landscaping conflicts including plants and a property line fence
  - No apparent utility conflicts

# LOCATION | PHOTOS



## Location J Field Notes

- **Sidewalk Continuity**
  - No sidewalk on north side of Badley
  - Existing sidewalk on south side of Badley from ~75 ft east of 3<sup>rd</sup> St to Creekside Arbour Apartments
  - No sidewalk either side of 3<sup>rd</sup>
- **Ada Compliance**
  - Existing directional ramps at SW and SE corners of Badley/4<sup>th</sup> and SW and SE corners of Badley/Janie are compliant
  - Existing sidewalk on Badley is ADA compliant
- **Street Lighting**
  - Street light at SW corner of Badley/3<sup>rd</sup>
  - Street light at SE corner of Badley/4<sup>th</sup>
  - Street light at SE corner of Badley/Janie
- **Other**
  - Existing rolled curb and gutter everywhere on Badley and 3<sup>rd</sup>
  - Minor landscaping conflicts including rock bed
  - Likely utility pole conflict at Badley/3<sup>rd</sup>



# LOCATION J PHOTOS



# LOCATION J PHOTOS



# LOCATION J PHOTOS



## Location K Field Notes

- **Sidewalk Continuity**
  - No sidewalk on either side of Washington except for 15' separated run at SE corner of Washington/2 ½ St
- **Ada Compliance**
  - Existing directional ramps at NW and NE corners of Washington/4<sup>th</sup> and SW and SE corners of Washington/2 ½ St are compliant
- **Street Lighting**
  - Street light at SW corner of Washington/2 ½
  - Street light midblock on north side of Washington
  - Street light at NE corner of Washington/4<sup>th</sup>
- **Other**
  - No existing curb and gutter except at existing ramps, separated sidewalk possible
  - Major utility conflicts on north side including inlet, several utility poles, possibly a fire hydrant, and drainage pipe
  - Conflict on south side with chain and pole type fence, but much less complex than north side

# LOCATION K PHOTOS



# LOCATION K PHOTOS



# LOCATION K PHOTOS



## Location L Field Notes

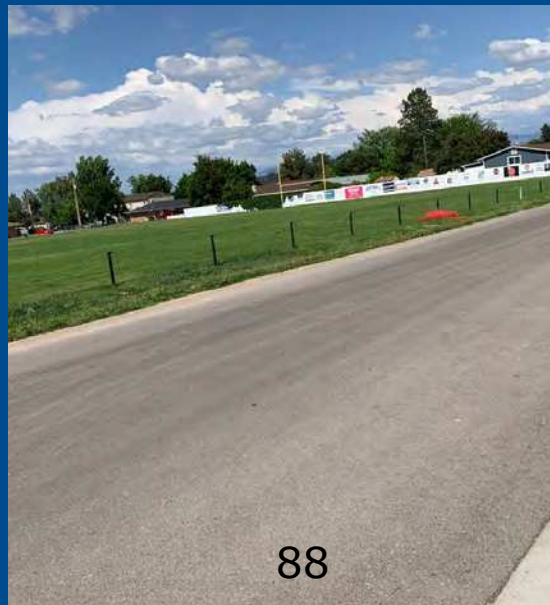
- **Sidewalk Continuity**
  - Sidewalk on south side of Carlton from 2 ½ St to 5<sup>th</sup> St
  - No sidewalk on north side of Carlton from 2 ½ St to 5<sup>th</sup> St
  - No sidewalk on either side of Carlton from 5<sup>th</sup> St to east end
- **Ada Compliance**
  - Existing ramps at NE corner of Carlton/2 ½, SE and SW corners of Carlton/3<sup>rd</sup>, SE and SW corners of Carlton/4<sup>th</sup> and SE, SW, NW, and NE corners of Carlton/5<sup>th</sup> are compliant
  - No ramp at SE corner of Carlton/2 ½
  - Existing ramp at NW corner of Carlton/2 ½ is not compliant (no detectable warning surface)
  - Existing sidewalk is ADA-compliant
- **Street Lighting**
  - Street light at south side of Carlton/2 ½
  - Street light at SE corner of Carlton/3<sup>rd</sup>
  - Street light at SE corner of Carlton/4<sup>th</sup>
  - Street light at NW corner of Carlton/5<sup>th</sup>
- **Other**
  - No existing curb and gutter on north side of Carlton from 2 ½ to 4<sup>th</sup>, separated sidewalk may be possible
  - Existing rolled curb and gutter everywhere else
  - Minor hardscaping (block retaining wall and curbing) and planting conflicts on north side, potential fire hydrant conflict on south side east of 5<sup>th</sup>



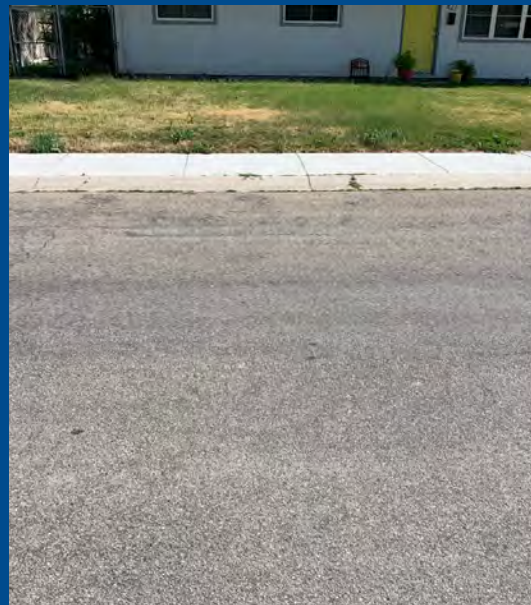
# LOCATION L PHOTOS



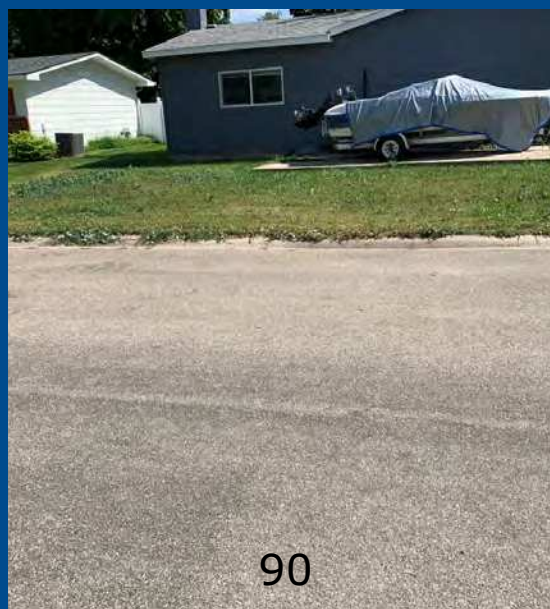
# LOCATION L PHOTOS



# LOCATION L PHOTOS



# LOCATION L PHOTOS



## Location M Field Notes

- **Sidewalk Continuity**
  - Existing sidewalk on south side from 2<sup>nd</sup> to east end
  - Existing sidewalk on north side from ~100 ft east of 4<sup>th</sup> to east end
  - No sidewalk on north side from ~100 ft east of 4<sup>th</sup> to 2<sup>nd</sup>
- **Ada Compliance**
  - Existing ramps at NE, NW, SW, and SE corners of State/2<sup>nd</sup>, SE and SW corners of State/3<sup>rd</sup>, SE and SW corners of State/4<sup>th</sup> and SE, SW, NW, and NE corners of State/5<sup>th</sup> are compliant
  - Existing sidewalk is ADA-compliant
- **Street Lighting**
  - Street light at NW corner of State/2<sup>nd</sup>
  - Street light at SE corner of State/3<sup>rd</sup>
  - Street light at NW corner of State/4<sup>th</sup>
  - Street light at SE corner of State/5<sup>th</sup>
- **Other**
  - Existing curb and gutter where existing sidewalk and ramps are, as well as other spot areas
  - Areas with no existing curb and gutter
  - Potential drain culvert conflict on north side east of 2<sup>nd</sup>
  - Landscaping conflicts (fence, plantings) on north side

# LOCATION M PHOTOS



# LOCATION M PHOTOS

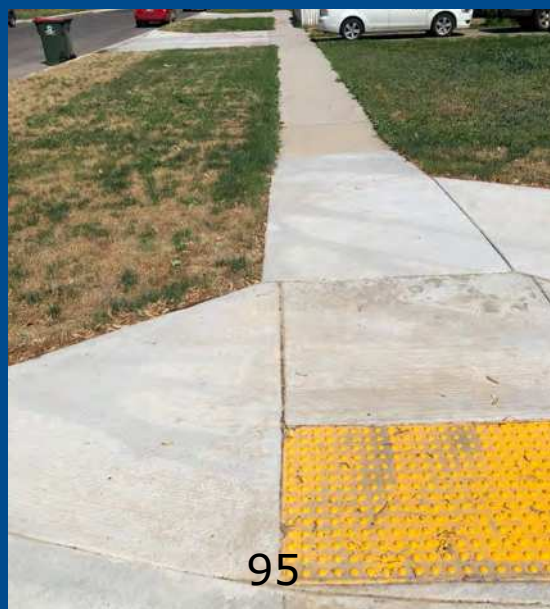


# LOCATION M PHOTOS





# LOCATION M PHOTOS



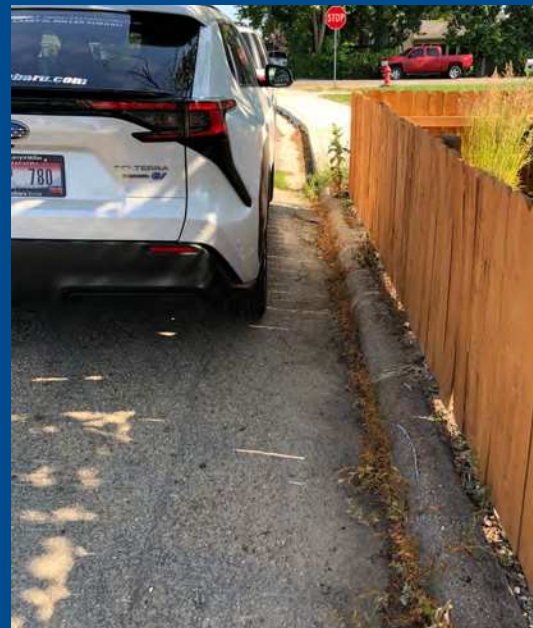
## Location N Field Notes

- **Sidewalk Continuity**
  - Existing separated sidewalk on west side from Pine to about 120' north of Pine
  - Existing sidewalk on east side from State to about 50 ft south of State
- **Ada Compliance**
  - Existing ramps at NE, NW, SW, and SE corners of 4<sup>th</sup>/Pine, SE and SW corners of 4<sup>th</sup>/State and SE and SW corners of 4<sup>th</sup>/Carlton are compliant
  - Existing sidewalk is ADA-compliant
- **Street Lighting**
  - Street light at NW corner of 4<sup>th</sup>/Pine
  - Street light at NW corner of 4<sup>th</sup>/State
  - Street light at SE corner of 4<sup>th</sup>/Carlton
- **Other**
  - Some isolated areas of curb and gutter, existing curb and gutter at east sidewalk and existing ramps
  - Separated walk possible on west side
  - Significant conflicts on east side including fences, inlets, utility pole
  - Utility pole conflict on west side but minor landscaping conflicts

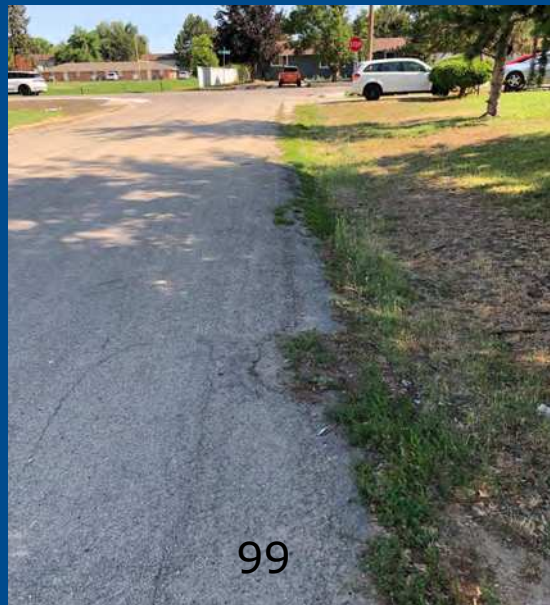
# LOCATION N PHOTOS



# LOCATION N PHOTOS



# LOCATION N PHOTOS



# LOCATION N PHOTOS



## Location O Field Notes

- **Sidewalk Continuity**
  - Existing continuous sidewalk (areas of separated and connected walk) on west side of 5<sup>th</sup>
  - Existing sidewalk on east side of 5<sup>th</sup> from State to ~100 ft south of State
- **Ada Compliance**
  - Existing ramps at SW and SE corners of 5<sup>th</sup>/Washington, NE, NW, SE and SW corners of 5<sup>th</sup>/Carlton, NE, NW, SE and SW corners of 5<sup>th</sup>/State and NE and NW corners of 5<sup>th</sup>/Pine are compliant
  - Existing sidewalk is ADA-compliant
- **Street Lighting**
  - Street light at SW corner of 5<sup>th</sup>/Washington
  - Street light at NW corner of 5<sup>th</sup>/Carlton
  - Street light at SE corner of 5<sup>th</sup>/State
  - Street light at NW corner of 5<sup>th</sup>/Pine
  - Pedestrian scale light at NE corner of 5<sup>th</sup>/Pine
- **Other**
  - No curb and gutter south of State
  - Rolled curb and gutter everywhere else
  - One utility pole conflict on east side, few other conflicts

# LOCATION O PHOTOS





# LOCATION O PHOTOS



# LOCATION O PHOTOS



# LOCATION O PHOTOS



## Location P Field Notes

- **Sidewalk Continuity**
  - No continuous sidewalk – there is a multi-use path to connect to north of this potential drain crossing
- **Ada Compliance**
  - Existing multi-use path is ADA-compliant
- **Street Lighting**
  - No street lighting
- **Other**
  - Existing structure over drain may potentially be used for a pedestrian crossing, connecting mobile home residents to multi-use path

# LOCATION P PHOTOS



## Location Q Field Notes

- **Sidewalk Continuity**
  - Existing sidewalk on north side of Pennwood
  - No sidewalk on south side of Pennwood
- **Ada Compliance**
  - Existing ramps at KOA RV park are non-compliant (no truncated domes)
  - Existing ramps at NW and SW corners of Pennwood/Meridian are compliant
  - Two mailboxes within clear width on north sidewalk
- **Street Lighting**
  - Street light on south side at east corner of Pennwood Apartments
- **Other**
  - Significant utility conflicts on south side near Meridian road – water, storm, fiber

# LOCATION Q PHOTOS



# LOCATION Q PHOTOS

